

HIGHWAY IMPROVEMENT DRAWINGS

PROPOSED GAS STATION & CONVENIENCE STORE

1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK
ULSTER COUNTY

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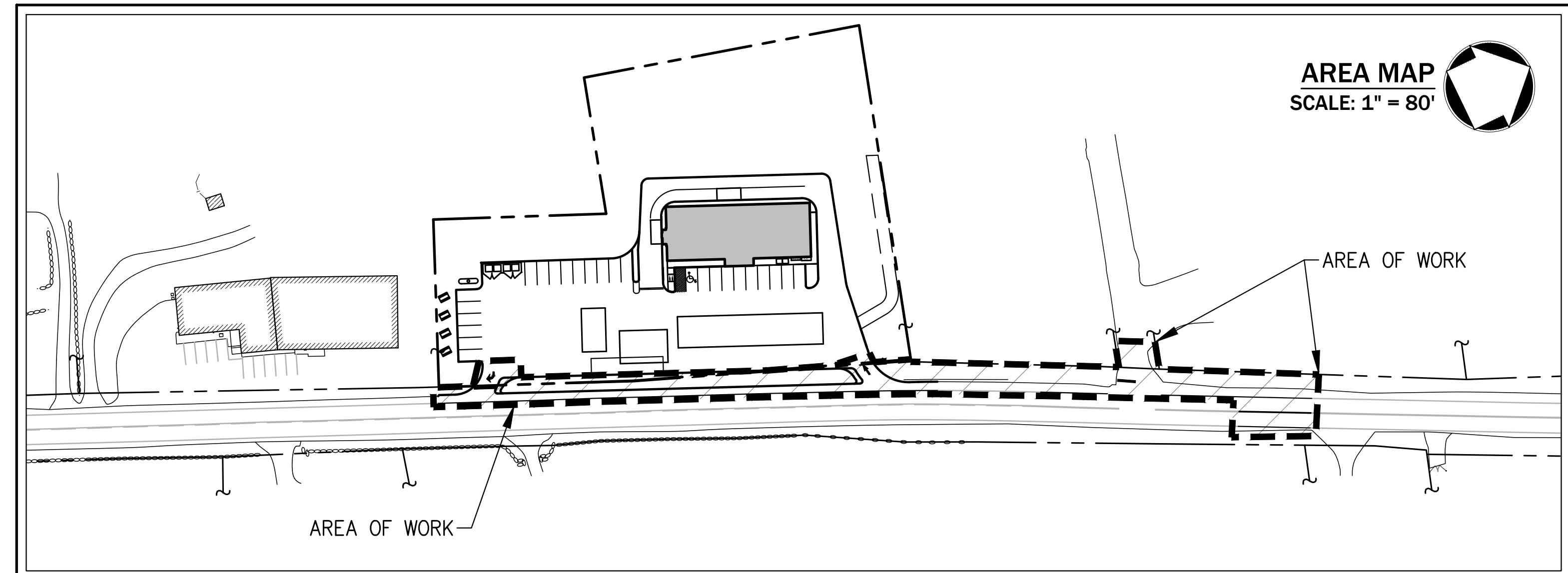
On-Site Civil Engineer:
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VICINITY MAP
SCALE: 1" = 1,000'



GENERAL NOTES:

- G14. CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED, OR THAT MAY BE DIRECTED BY THE ENGINEER, TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR UTILITIES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE SUM BID PRICE FOR THE VARIOUS ITEMS IN THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- G15. CONTRACTOR SHALL PROVIDE SAFE AND ADEQUATE ACCESS INTO OR THROUGH THE WORK SITE BY EMERGENCY VEHICLES AT ALL TIMES.
- G16. ALL EXISTING PAVEMENT AND SHOULDER SURFACES THAT ARE TO BE RESURFACED SHALL BE CLEANED IN ACCORDANCE WITH SECTION 633 (CONDITIONING OF EXISTING PAVEMENT) OF THE NYSDOT STANDARD SPECIFICATIONS.
- G17. TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL MEASURES, ACCORDING TO NYSDOT STANDARD SPEC SECTION 209, SHALL BE USED DURING THE LIFE OF THE CONTRACT.
- G18. WHEN CONSTRUCTION ACTIVITY IS EXPECTED TO BE AT OR WITHIN 3 FEET OF THE EXISTING RIGHT-OF-WAY LIMITS, THE CONTRACTOR SHALL HAVE THE EXISTING RIGHT-OF-WAY MARKED IN THE FIELD BY A LICENSED SURVEYOR.
- G19. PRIOR TO COMMENCEMENT OF ANY WORK, THE CONTRACTOR SHALL SUBMIT A PROPOSED CONSTRUCTION SEQUENCE TO THE ENGINEER FOR APPROVAL BY THE APPROPRIATE APPROVAL AUTHORITIES.
- G20. ALL WORK SHALL BE DONE IN STRICT COMPLIANCE WITH ALL APPLICABLE NATIONAL, STATE, AND LOCAL CODES, STANDARDS, ORDINANCES, RULES, AND REGULATIONS. ALL CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL SAFETY CODES. APPLICABLE SAFETY CODES MEAN THE LATEST EDITIONS OF THE APPROPRIATE AMERICAN NATIONAL STANDARDS INSTITUTE, TO THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION, AND THE STATE, SAFETY AND HEALTH STANDARDS (OSHA), AND APPLICABLE SAFETY, HEALTH REGULATIONS, AND BUILDING CODES FOR CONSTRUCTION IN THE STATE OF NEW YORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GUARDING AND PROTECTING ALL OPEN EXCAVATIONS IN ACCORDANCE WITH THE PROVISION OF SECTION 107-05 (SAFETY AND HEALTH REQUIREMENTS) OF THE NYSDOT STANDARD SPECIFICATIONS. IF THE CONTRACTOR PERFORMS ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THE AFFECTED AREA SHALL BE DISCONTINUED AND IMMEDIATE ACTION SHALL BE TAKEN TO CORRECT THE SITUATION TO THE SATISFACTION OF THE APPROVAL AUTHORITY HAVING JURISDICTION.
- G21. POSITIVE DRAINSAGE SHALL BE PROVIDED TO ALL DRAINAGE STRUCTURES. ALL PRE-EXISTING, NEWLY INSTALLED BASINS AND PIPES SHALL BE CLEARED OUT OF ALL DEBRIS, AS REQUIRED, TO ENSURE NON-RESTRICTED FLOW PRIOR TO COMPLETION OF WORK.
- G22. CONTRACTOR SHALL NOTIFY NYSDOT AND THE TOWN OF MARLBOROUGH, AS APPLICABLE, 72 HOURS PRIOR TO ANY STREET OR LANE CLOSURES OR ANY WORK AFFECTING TRAFFIC SIGNALS.
- G23. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL COMMERCIAL AND RESIDENTIAL PROPERTIES AT ALL TIMES. THE DISTRIBUTION OF EXCAVATED MATERIAL, CONSTRUCTION WASTE, AND OTHER MATERIALS MAY BE PLACED WITH SUBSURFACE MATERIAL, EXCEPT THAT TEMPORARY ASPHALT CONCRETE SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE PEDESTRIAN ACCESS AT ALL TIMES.
- G24. CONTRACTOR SHALL MAINTAIN THE INTEGRITY OF EXISTING PAVEMENT TO REMAIN.
- G25. CONTRACTOR TO RESTORE PAVEMENT SURFACE AND PAVEMENT MARKINGS IN KIND AS NECESSARY FOR AREAS OF PAVEMENT REPLACEMENT.
- G26. CONTRACTOR SHALL NOTIFY ALL RESIDENTS AND BUSINESSES LOCATED IN THE WORK ZONE IN WRITING AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. SUCH NOTICES WILL BE FURNISHED BY THE CONTRACTOR AND APPROVED BY THE NYSDOT AND THE TOWN OF MARLBOROUGH, AS APPLICABLE.
- G27. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SURFACE DRAINAGE DURING THE COURSE OF WORK.
- G28. WHEN BACKFILLING AROUND PROPOSED OR EXISTING STRUCTURES, COMPACTION EFFORTS SHALL CONFORM TO SECTION 203-3.13 TO 203-3.15 OF THE NYSDOT STANDARD SPECIFICATIONS.
- G29. SUBMITTALS, CATALOG CUTS, SAMPLES, AND SHOP DRAWINGS MUST BE RECEIVED, REVIEWED AND APPROVED BY THE NYSDOT AND THE TOWN OF MARLBOROUGH, AS APPLICABLE, PRIOR TO ORDERING OR FABRICATING OF MATERIAL AND PRIOR TO INSTALLATION OF MATERIAL. FAILURE TO DO SO MAY BE CAUSE FOR TIME DELAYS IN INSTALLATION OF THE CONTRACTOR. THE CONTRACTOR WILL NOT BE ENTITLED TO COMPENSATION FOR SAME TIME DELAYS, REMOVALS, OR REPLACEMENTS.
- G30. DURING CONSTRUCTION, NO WET OR FRESH CONCRETE OR LEACHATE SHALL BE ALLOWED TO ESCAPE INTO THE WATERS OF NEW YORK STATE, NOR SHALL WASHINGS FROM READY-MIX TRUCKS, MIXERS OR OTHER DEVICES BE ALLOWED TO ENTER ANY STORMWATER COLLECTION SYSTEM.
- G31. AS-BUILT DRAWINGS SHALL BE PROVIDED BY THE CONTRACTOR.
- G32. COPIES OF PLANS AND ALL REQUIRED PERMITS SHALL BE KEPT ON THE WORK SITE AT ALL TIMES.

JMC Drawing List:

- HP-1 COVER SHEET
HP-2 LAYOUT PLAN
HP-3 SIGNING & STRIPING PLAN
HP-4 GRADING & UTILITIES PLAN
HP-5 EROSION AND SEDIMENT CONTROL PLAN
HP-6 WORK ZONE TRAFFIC CONTROL PLAN
HP-7 NYSDOT DETAILS
HP-8 NYSDOT DETAILS
HP-9 NYSDOT DETAILS
HP-10 NYSDOT DETAILS
HP-11 NYSDOT DETAILS

EXCAVATION SPECIAL NOTES:

1. IF EXCAVATION COMES WITHIN A ONE ON ONE PLANE TO THE EDGE OF PAVEMENT THEN "SHEETING" SHALL BE USED TO PREVENT UNDERMINING OF THE PAVEMENT.
2. ANY EXCAVATION OF FIVE (5) FOOT DEPTH OR MORE REQUIRES A SHEETING DETAIL DESIGNED, STAMPED, AND SIGNED BY A NEW YORK STATE LICENSED PROFESSIONAL ENGINEER.
3. IF A PREFABRICATED "SHEETING BOX" IS TO BE USED, THERE ARE DESIGN PLANS FOR THESE ALSO. IF NOT STAMPED AND SIGNED BY A NEW YORK STATE LICENSED PROFESSIONAL ENGINEER, YOU MUST HAVE ONE REVIEW THE DESIGN, STAMP AND SIGN.
4. TRANSVERSE UTILITY CROSSINGS UNDER NYSDOT PAVEMENT SHALL BE A MINIMUM OF FIVE (5) FOOT BETWEEN THE TOP OF PAVEMENT AND TOP OF UTILITY.
5. LONGITUDINAL UTILITY RUNS, OUTSIDE THE PAVEMENT LIMITS MUST BE A MINIMUM OF THREE (3) FOOT TOP OF GRADE TO TOP OF UTILITY. THE PAVEMENT LIMITS ARE 50 FEET FROM THE HIGHWAY CENTER LINE.
6. LONGITUDINAL UTILITY RUNS SHOULD BE KEPT AS CLOSE TO THE RIGHT OF WAY LINE OR AS FAR FROM THE PAVEMENT AS POSSIBLE.

SNOW PLOWING REQUIREMENTS:

DOUE TO POSSIBLE SNOW FALL AND HENCE SNOW PLOWING OPERATIONS, ANY STEEL PLATES USED TO COVER AN EXCAVATION SHALL BE RECESSED INTO THE PAVEMENT AND FLINED.

WINTER TIME EARTHWORK REQUIREMENTS:

ALL PERMIT WORK UNDER CONSTRUCTION BETWEEN THE DATES OF NOVEMBER 1 THROUGH MAY 1 SHALL CONFORM TO THE FOLLOWING REQUIREMENTS:

1. GRANULAR OR OTHER FROST SUSCEPTIBLE MATERIAL SHALL NOT BE PLACED WHEN THE TEMPERATURE IS BELOW 32 DEGREES FAHRENHEIT.
2. FROZEN MATERIAL SHALL NOT BE INCORPORATED INTO EMBANKMENTS OR BACKFILLS.
3. MATERIAL SHALL NOT BE PLACED ON FROZEN GROUND.
4. THE MATERIAL SHALL BE COMPAKTED IN ACCORDANCE WITH HIGHWAY DESIGN MANUAL CHAPTER 13 APPENDIX 13C, "REQUIREMENTS FOR THE DESIGN AND CONSTRUCTION OF UNDERGROUND UTILITY INSTALLATIONS WITHIN THE STATE HIGHWAY RIGHT-OF-WAY" (AKA BLUE BOOK), LATEST REVISION, AND/OR E10-015 (ENGINEERING INSTRUCTION).
5. ANY SPECIAL CONDITIONS AND RESTRICTIONS AS MAY BE IMPOSED BY THE RESIDENT ENGINEER OR THE REGIONAL GEOTECHNICAL (SOILS) ENGINEER.

PAVEMENT REPLACEMENT REQUIREMENTS:

DOUE TO ASPHALT PLANTS CLOSED IN THE WINTER, TEMPORARY PAVEMENT REPLACEMENTS SHALL BE AS PER THE NYSDOT PERMIT FIELD ENGINEER (AKA INSPECTOR) AND/OR RESIDENT ENGINEER. PERMITTEE MUST MONITOR AND MAINTAIN THE TEMPORARY PAVEMENT REPLACEMENT.

TEMPORARY PAVEMENT REPLACEMENTS ARE REQUIRED TO BE REPLACED IN-KIND IN THE SPRING WHEN THE WEATHER ALLOWS NYSDOT STANDARD SPECIFICATIONS TO BE MET.

No.	Revision	Date	By
1.	REVISED FOR BIDDING PURPOSES	07/12/2018	KRM
2.	REVISED LAYOUT	05/06/2019	KRM
3.	REVISED PER NYSDOT COMMENTS	10/10/2019	KRM

Previous Editions Obsolete

SUBSURFACE UTILITY LOCATIONS ARE BASED ON A COMPILATION OF FIELD EVIDENCE, AVAILABLE RECORD PLANS AND/OR UTILITY MARK-OUTS. THE LOCATION OR COMPLETENESS OF UNDERGROUND INFORMATION CANNOT BE GUARANTEED. VERIFY THE ACTUAL LOCATION OF ALL UTILITIES PRIOR TO EXCAVATION OR CONSTRUCTION.



Know what's below.
Call before you dig.

ANY ALTERATION OF PLANS, SPECIFICATIONS, PLATS AND REPORTS BEARING THE SEAL OF A LICENSED PROFESSIONAL ENGINEER OR LICENSED LAND SURVEYOR IS A VIOLATION OF SECTION 720 OF THE NEW YORK STATE EDUCATION LAW, EXCEPT AS PROVIDED FOR BY SECTION 720, SUBSECTION 2.

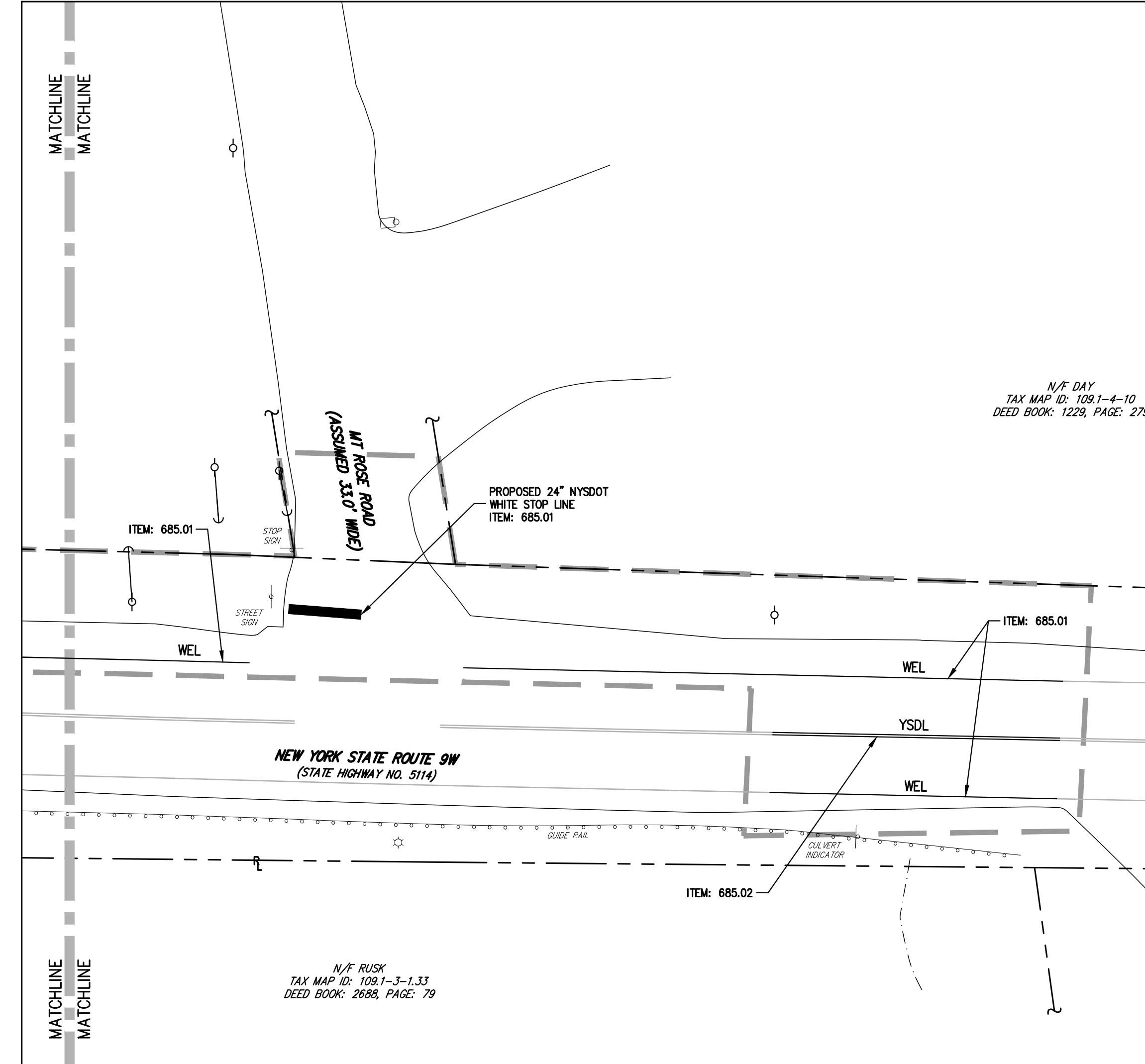
Drawn: KRM Approved: RJP
Scale: AS SHOWN
Date: 04/27/2018
Project No: 15119
1519-HIGHWAY COVER 9.HWY-COVER.sdr
Drawing No: HP-1



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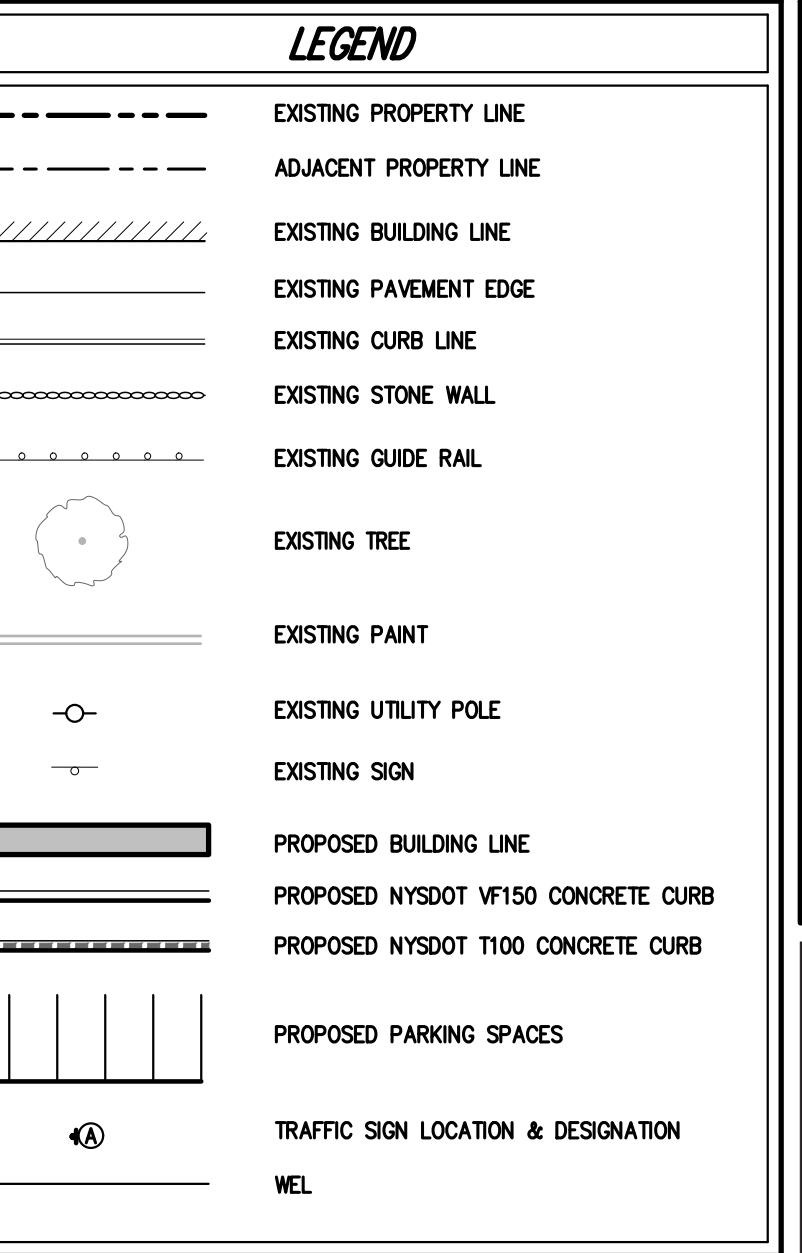
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SIGN TABLE

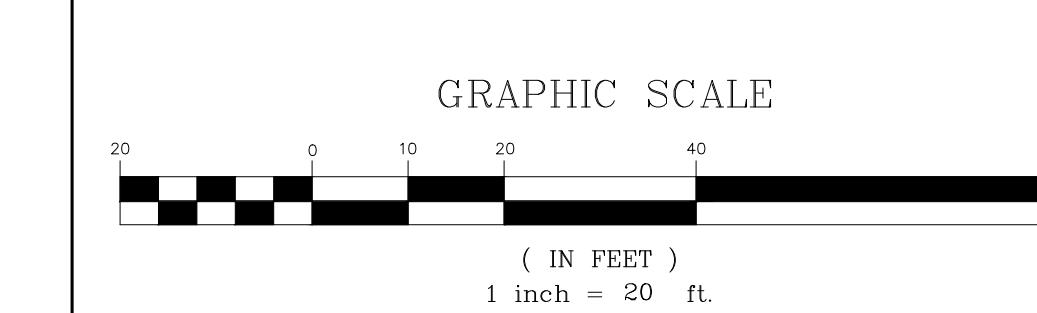
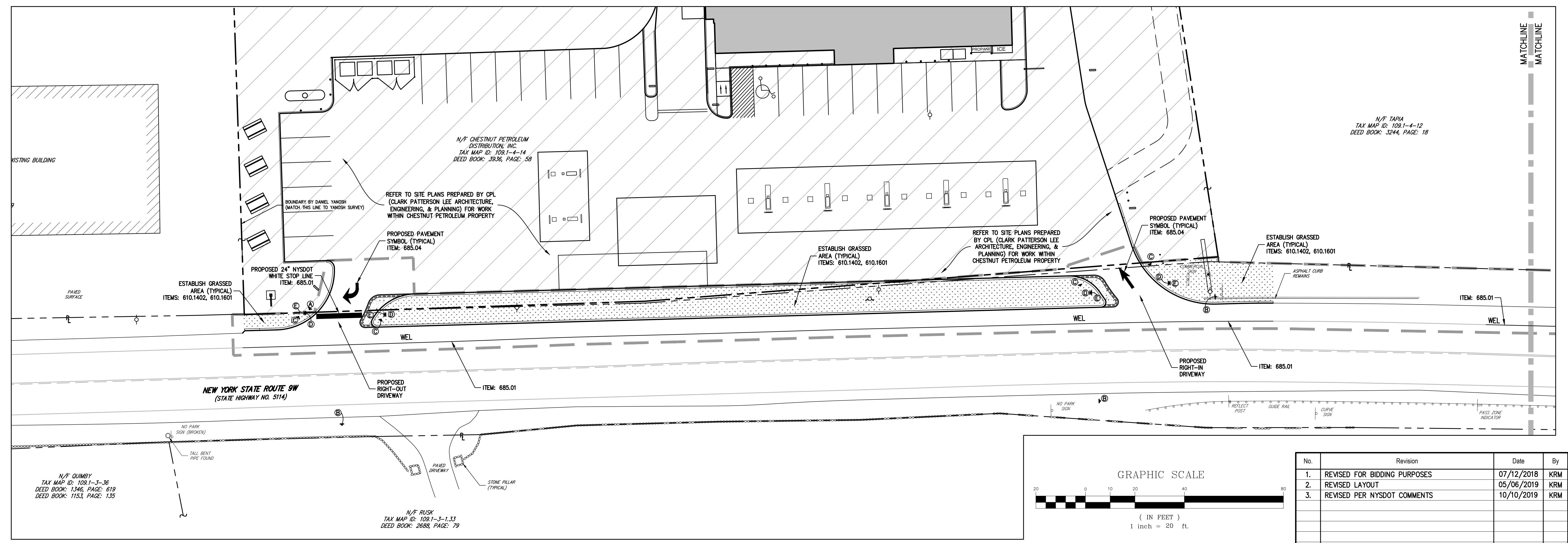
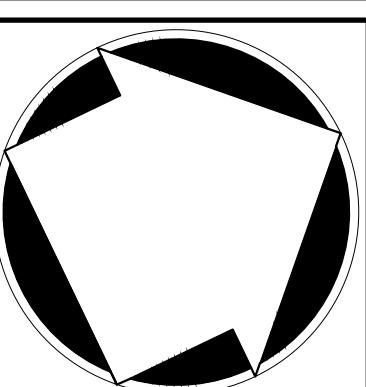
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A	STOP	30"X30" 24"X24"	WHITE ON RED BLACK ON RED AND WHITE	STEEL CHANNEL ITEM: 645.81	7'-0"	R1-1 R3-2	ITEM: 645.5202
B	STOP	24"X24"	BLACK ON RED AND WHITE	STEEL CHANNEL ITEM: 645.81	7'-0"	R3-2	ITEM: 645.5202
C	DO NOT ENTER	30"X30"	RED ON WHITE	STEEL CHANNEL ITEM: 645.81	7'-0"	R5-1	ITEM: 645.5102
D	ONE WAY	36"X12"	WHITE ARROW ON BLACK	TWO STEEL CHANNELS ITEM: 645.81	7'-0"	R6-1L	ITEM: 645.5102
E	ONE WAY	36"X12"	WHITE ARROW ON BLACK	TWO STEEL CHANNELS ITEM: 645.81	7'-0"	R6-1R	ITEM: 645.5102



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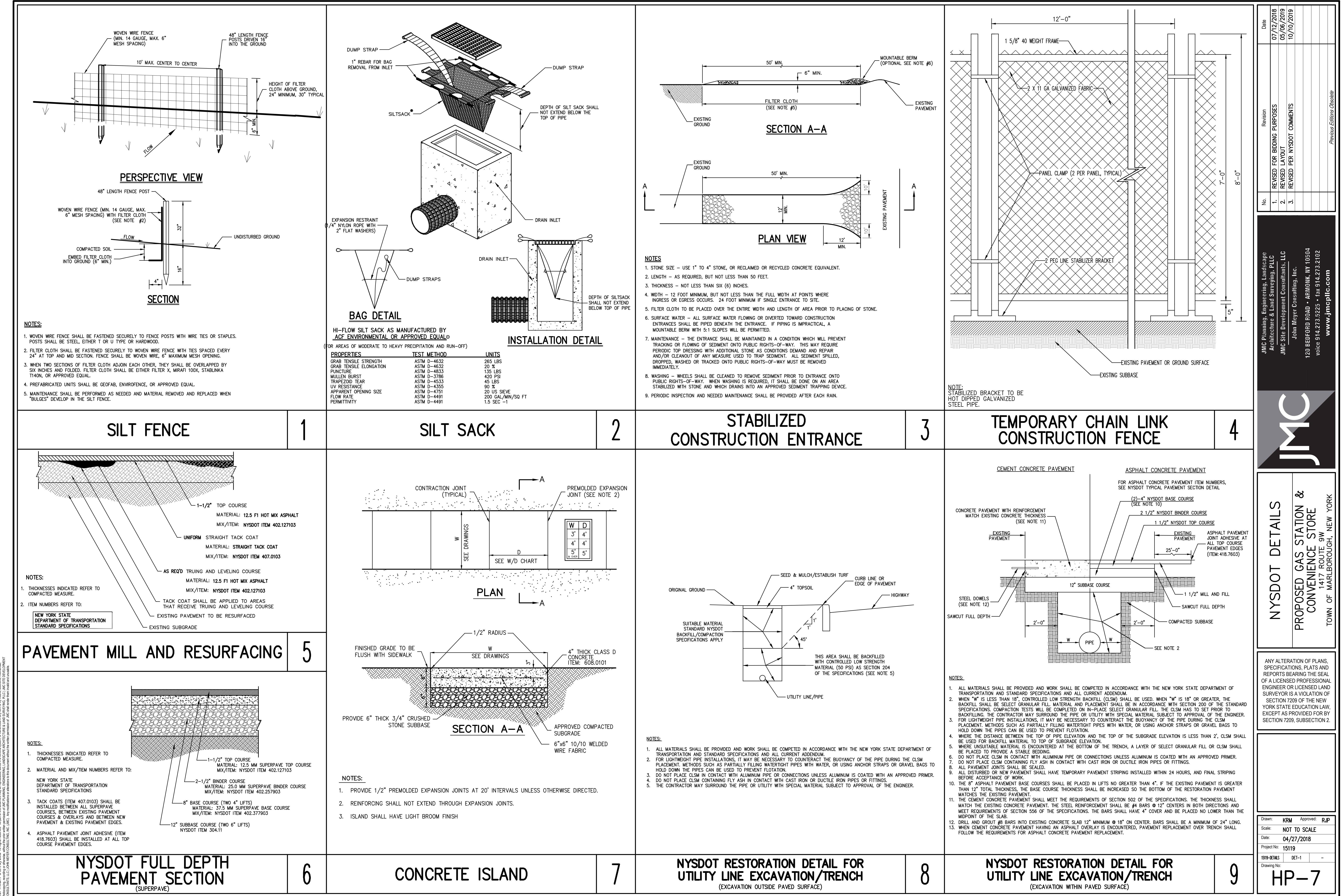


No.	Revision	Date	By
1.	REVISED FOR BIDDING PURPOSES	07/12/2018	KRM
2.	REVISED LAYOUT	05/06/2019	KRM
3.	REVISED PER NYSDOT COMMENTS	10/10/2019	KRM

HP-3

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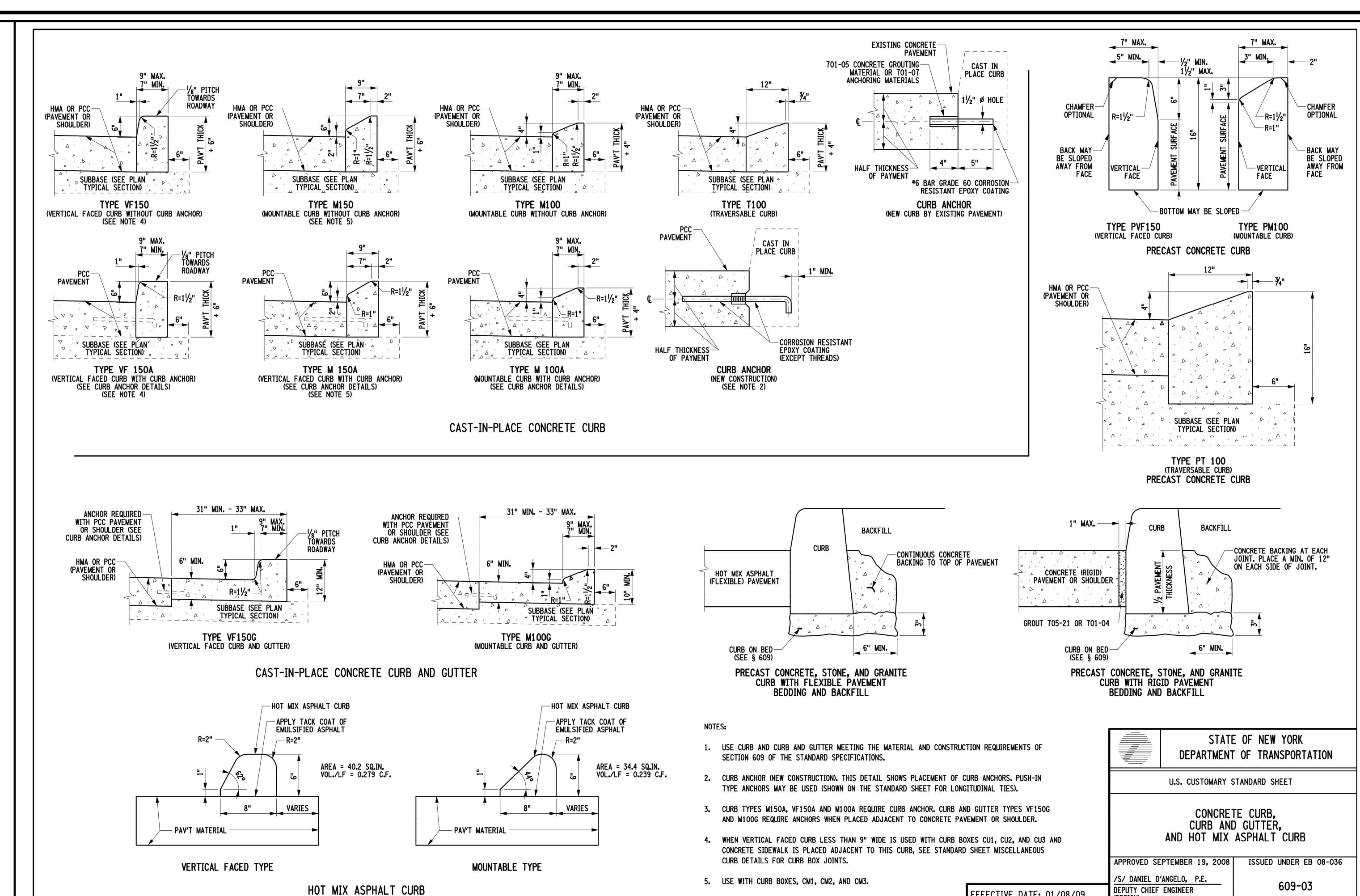
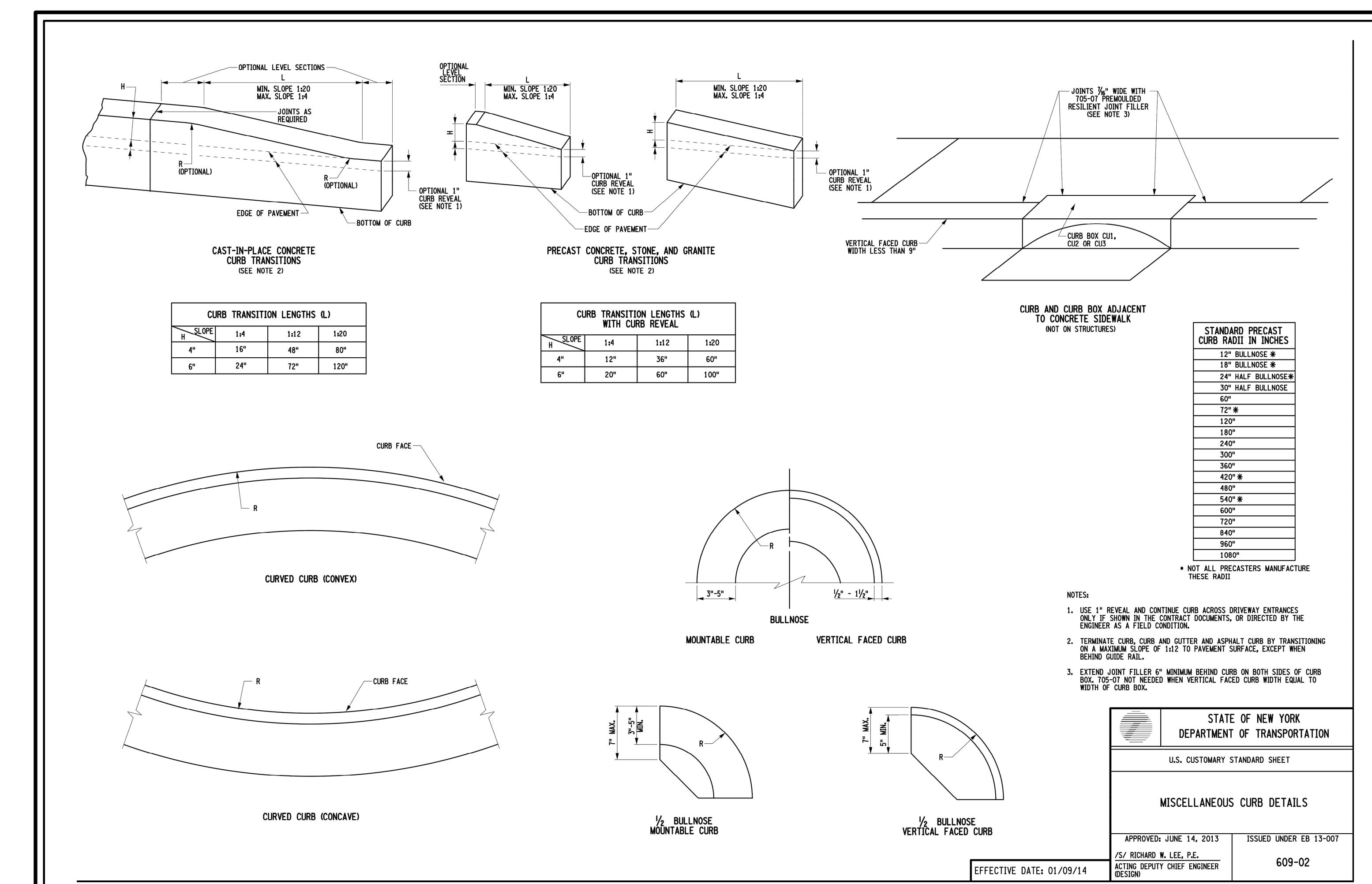
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Scale: 1" = 20'
Date: 04/27/2018
Project No: 15119
15119-HIGHWAY STRIPING JMC-STRIPING.dwg
Drawing No:



NYSDOT DETAILS
PROPOSED GAS STATION & CONVENIENCE STORE
1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK

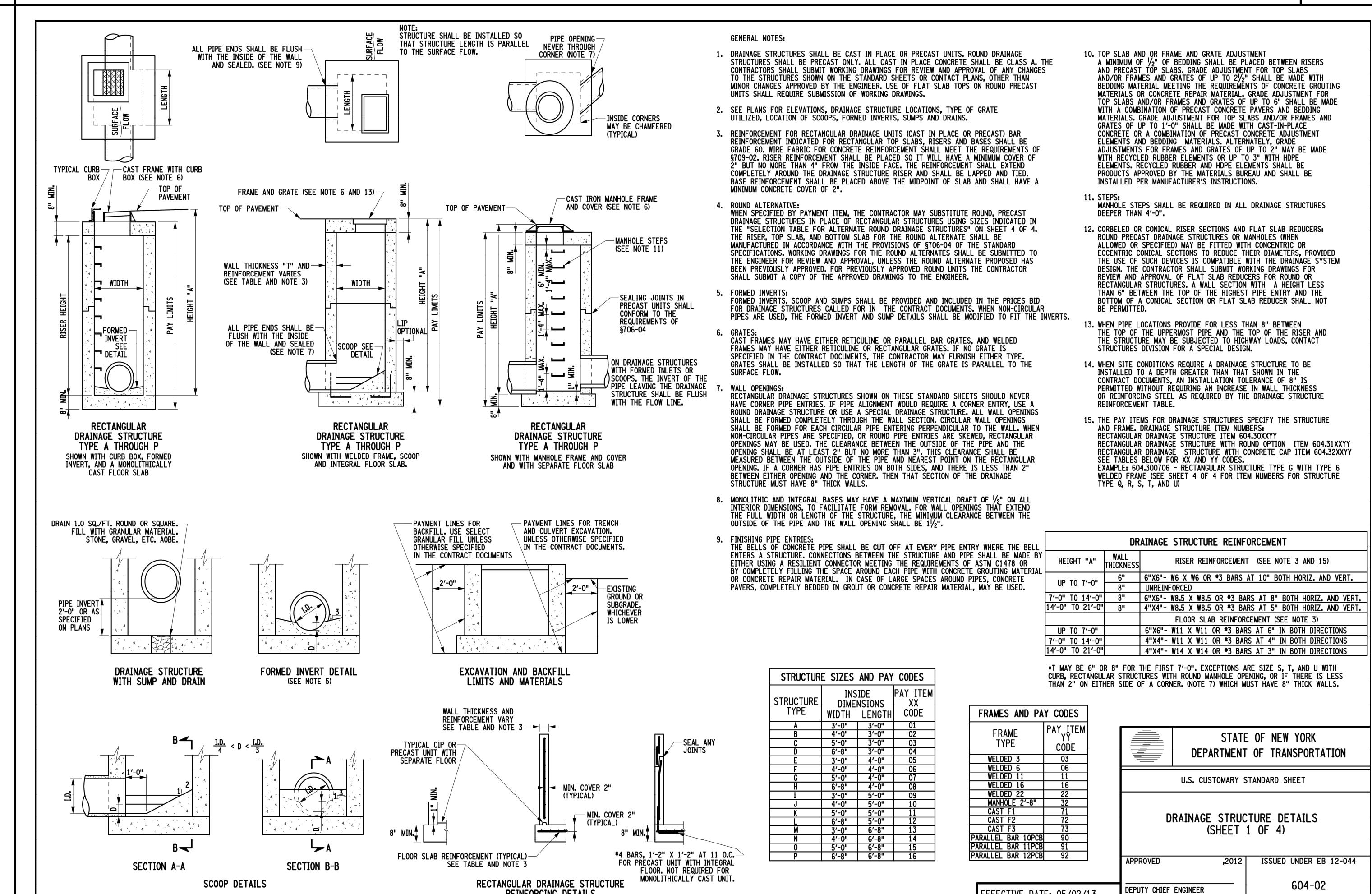
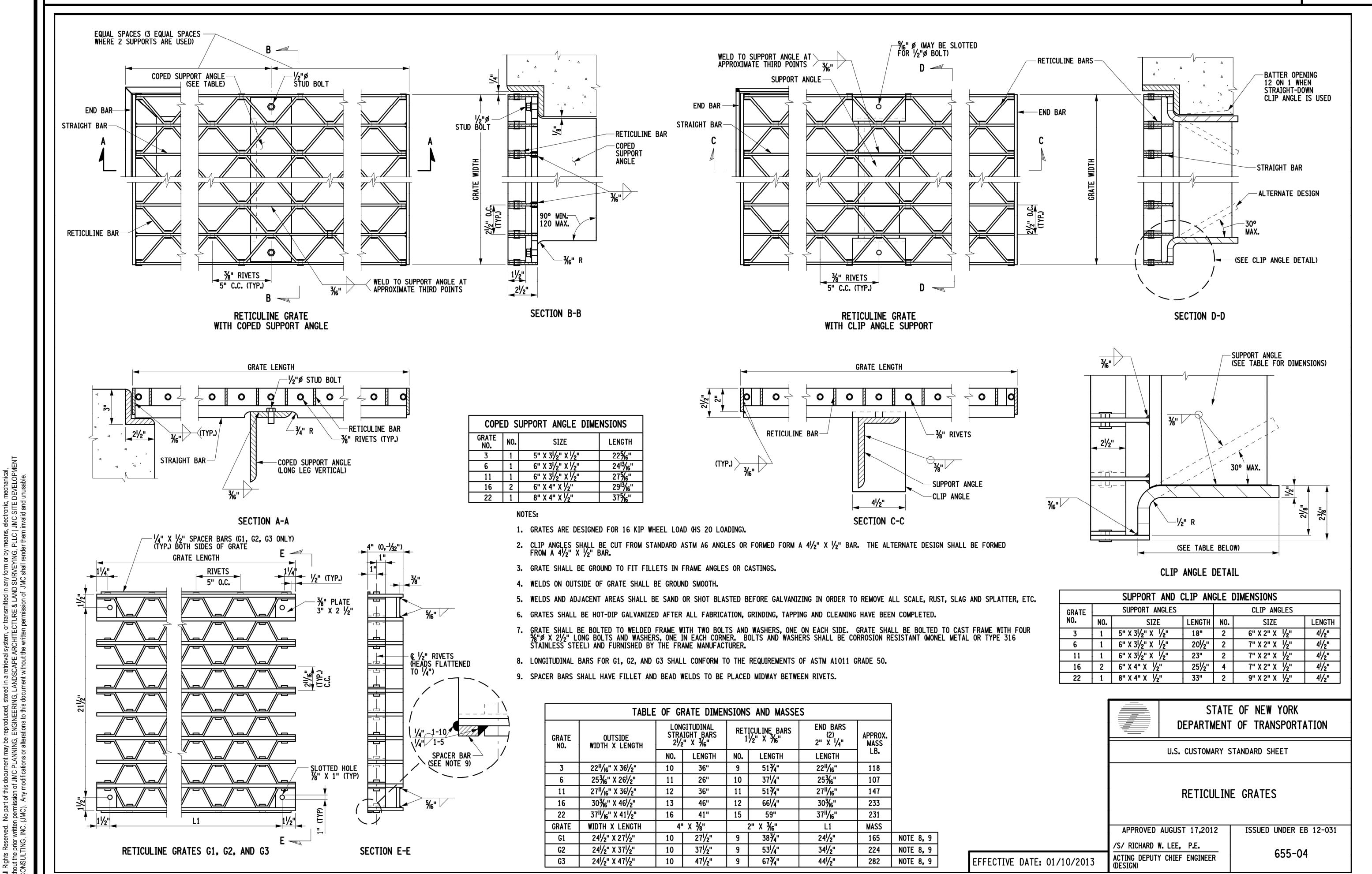
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Date: 04/27/2018
Project No: 15119
15119-DETAILS DET-1 -
Drawing No: HP-7



MISCELLANEOUS CURB

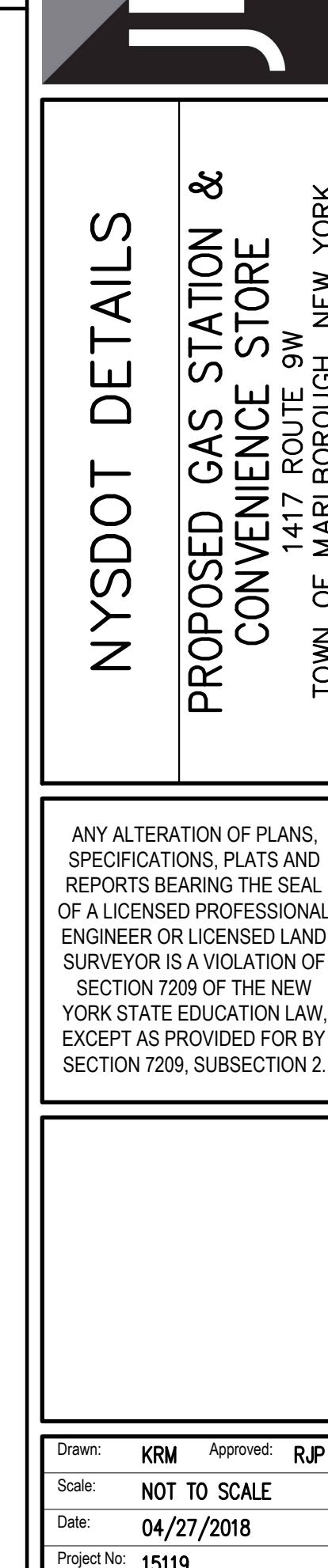
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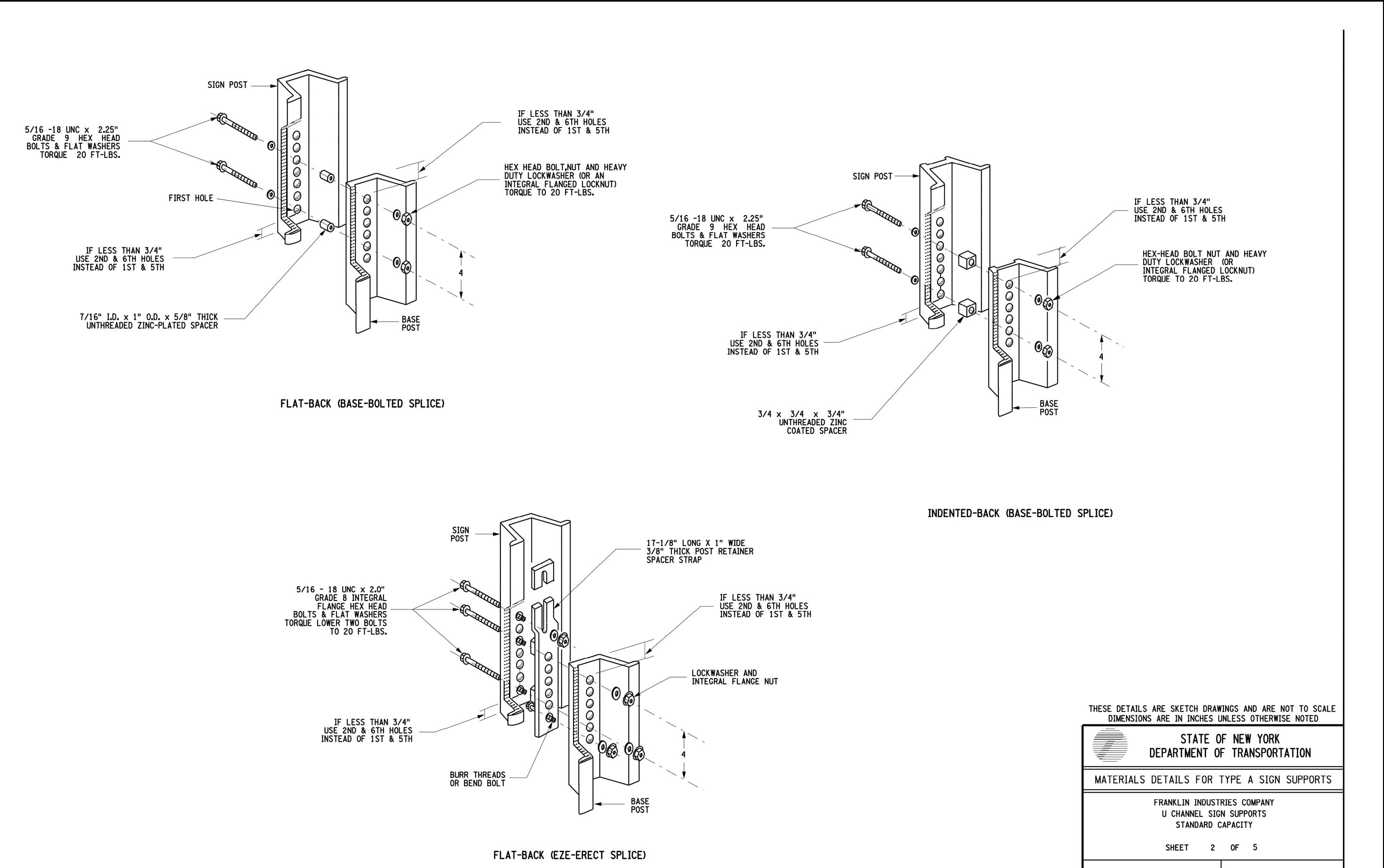
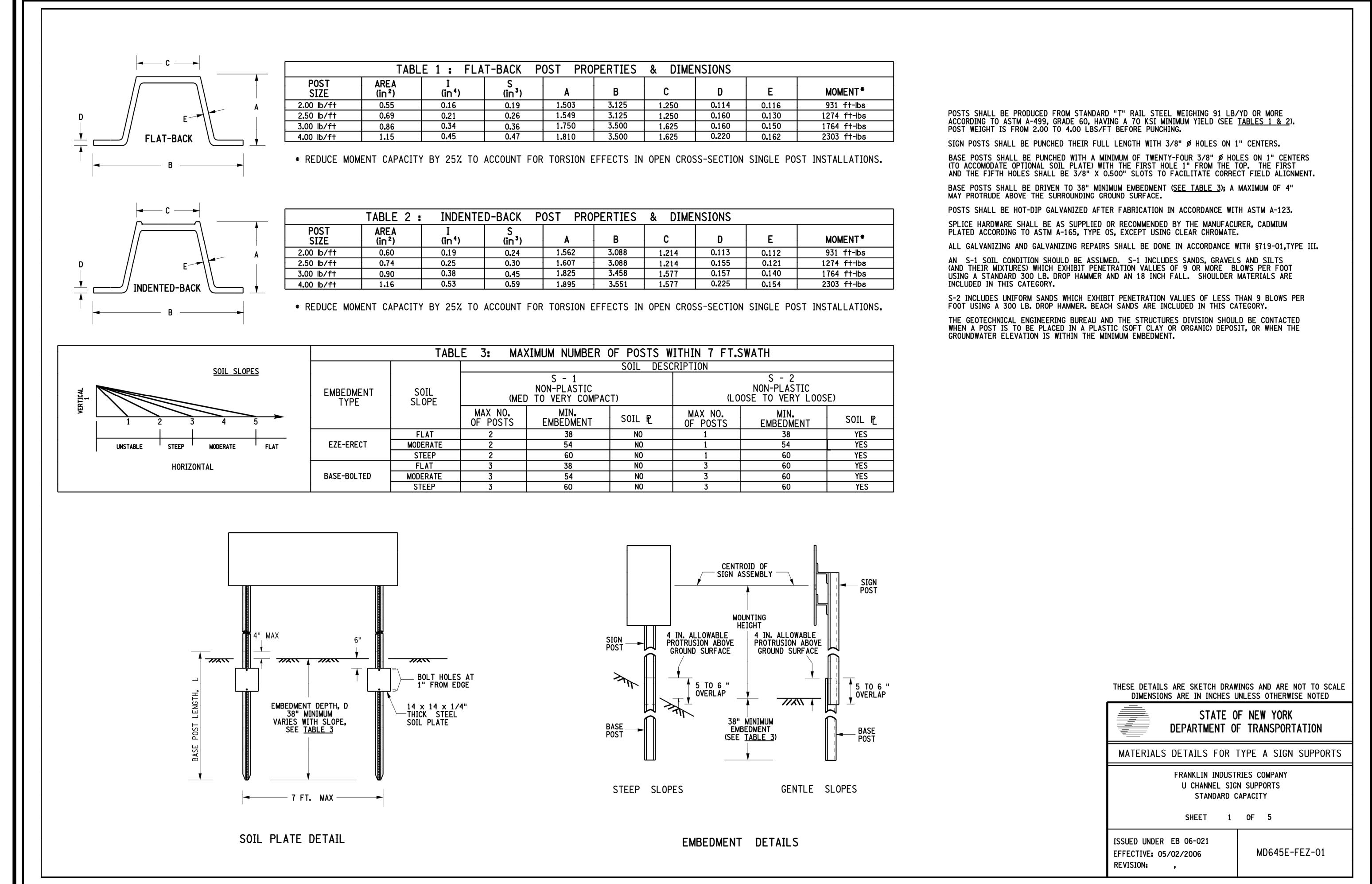
NYSDOT RETICULINE GRATES

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NYSDOT DRAINAGE STRUCTURE



HP-8



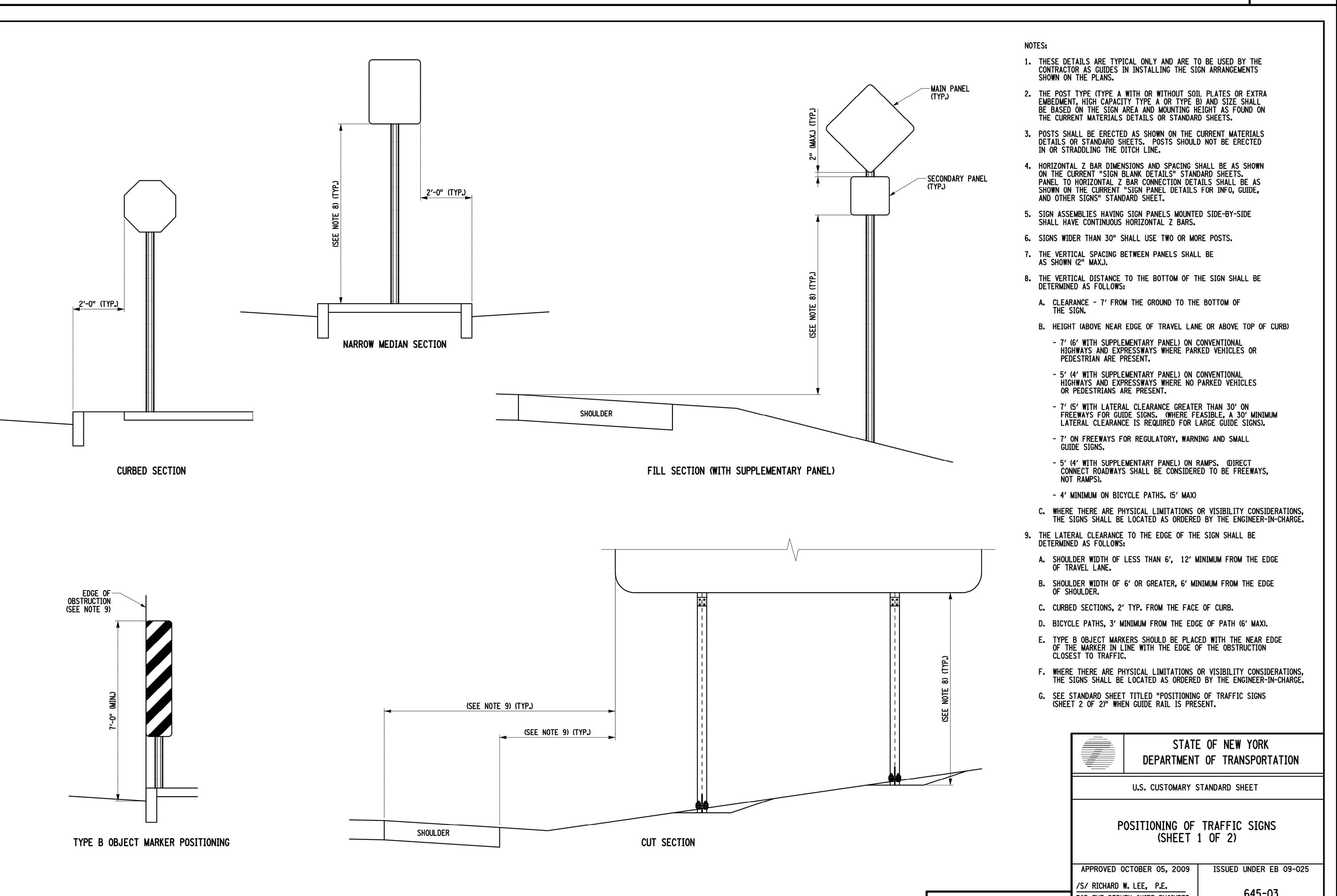
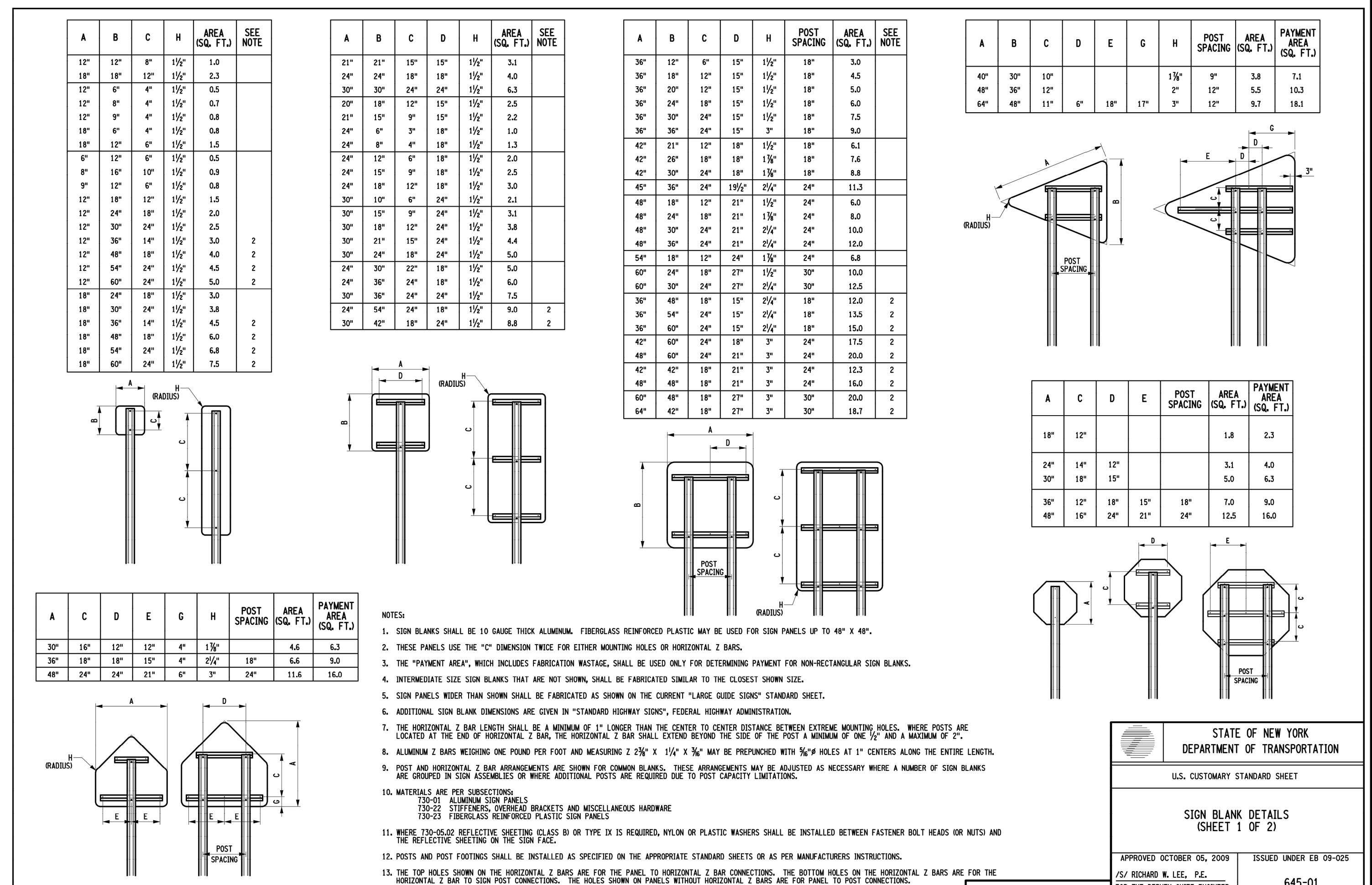
No.	1. REVISED FOR BIDDING PURPOSES
	2. REVISED LAYOUT
	3. REVISED PER NYSDOT COMMENTS

Previous Editions Obsolete

Date 07/12/2018
05/06/2019
10/10/2019

TYPE A SIGN SUPPORTS

18



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Scale: NOT TO SCALE	
Date: 04/27/2018	
Project No: 15119	
15119-DETAILS	DET-4
Drawing No:	HP-10

SIGN BLANK DETAILS

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POSITIONING OF TRAFFIC SIGNS

