

**1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK
ULSTER COUNTY**

JMC JMC Planning, Engineering, Landscape
Architecture & Land Surveying, PLLC
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John Meyer Consulting, Inc.
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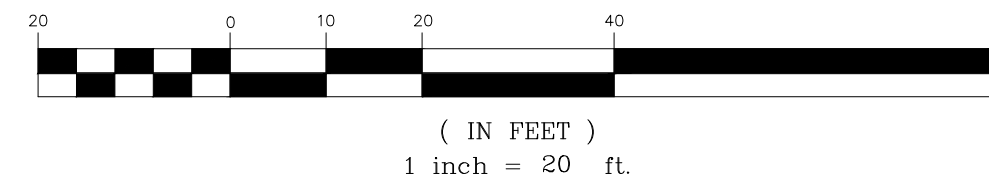
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- P1. PRIOR TO STARTING ANY PAVING, THE CONTRACTOR SHALL COORDINATE A PRE-PAVING MEETING WITH THE NYSDOT, PROPERTY OWNER, ENGINEER OF RECORD, PAVING CONTRACTOR AND TESTING CONTRACTOR.
- P2. ALL PAVING SHALL BE DONE IN ACCORDANCE WITH NEW YORK STATE DEPARTMENT OF TRANSPORTATION REQUIREMENTS AND SPECIFICATIONS.
- P3. TOP COATS SHALL BE INSTALLED BETWEEN ALL SUPERPAVE COURSES, BETWEEN EXISTING PAVEMENT SURFACES & OVERLAYS AND BETWEEN NEW PAVEMENT & EXISTING PAVEMENT EDGES.
- P4. PLACEMENT OF PROPOSED FULL DEPTH ASPHALT PAVEMENT IN WIDENING AREAS SHALL BE LIMITED TO WIDTHS OF 4 FEET OR GREATER AND SHALL BE COMPLETED WITH A WALK BEHIND ROLLER OR LARGER PIECE OF EQUIPMENT. HAND COMPACTION AND JUMPING JACKS SHALL NOT BE USED.
- P5. FOR WIDENING AREAS LESS THAN 4 FEET, CONTRACTOR SHALL FILL AREA WITH CLASS C CONCRETE AND PROVIDE 1-1/2" TOP SURFACE OF ASPHALT PAVEMENT ON TOP OF ROUGH CONCRETE SURFACE. ITEMS 402.127/103, 407.0103, 503.0101.
- P6. CONTRACTOR SHALL HAVE A WATER TRUCK ON SITE DURING PAVING ACTIVITIES AND SHALL WET AND COOL PAVEMENT AS NECESSARY, AS DIRECTED BY PAVEMENT TESTING TECHNICIAN.

NOTES:

1. EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM A SURVEY TITLED, "RIGHT OF WAY & TOPOGRAPHIC SURVEY," PREPARED BY ENGINEERING & SURVEYING PROPERTIES, REVISED 04/18/2018.
2. NYS ROUTE 9W DESIGN SPEED IS 50 MPH.

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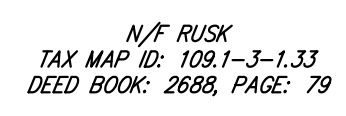
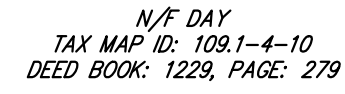
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LAYOUT PLAN

PROPOSED GAS STATION &
CONVENIENCE STORE
1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK

<div style="border: 1px solid black; height: 150px; margin-bottom: 5px;"></div>		
Drawn:	KRM	Approved: RJP
Scale:	1" = 20'	
Date:	04/27/2018	
Project No:	15119	
1510-HIGHWAY	LAYOUT	LJH-W-LAY.scr
Drawing No: HP-2		

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1. EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM A SURVEY TITLED, "RIGHT OF WAY & TOPOGRAPHIC SURVEY," PREPARED BY ENGINEERING & SURVEYING PROPERTIES, REVISED 04/18/2018.
2. NYS ROUTE 9W DESIGN SPEED IS 50 MPH.

WEL - 4" WIDE WHITE EDGE LINE
YSDL - 2-4" WIDE YELLOW SOLID DOUBLE LINE SPACED 8" ON CENTER

Date
07/12/2018
05/06/2019
10/10/2019

Drawn:	KRM	Approved:	RJP
Scale:	1" = 20'		
Date:	04/27/2018		
Project No:	15119		
1519-HIGHWAY	STRIPING	L-HWY-STRIPING	
Drawing No:			
HP-3			

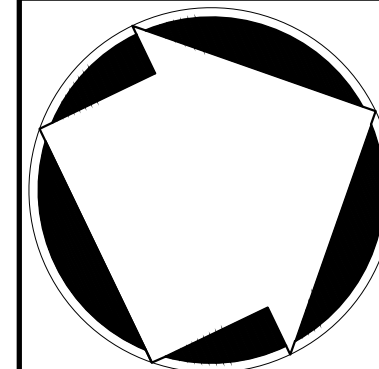
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**SIGNING &
STRIPING PLAN**

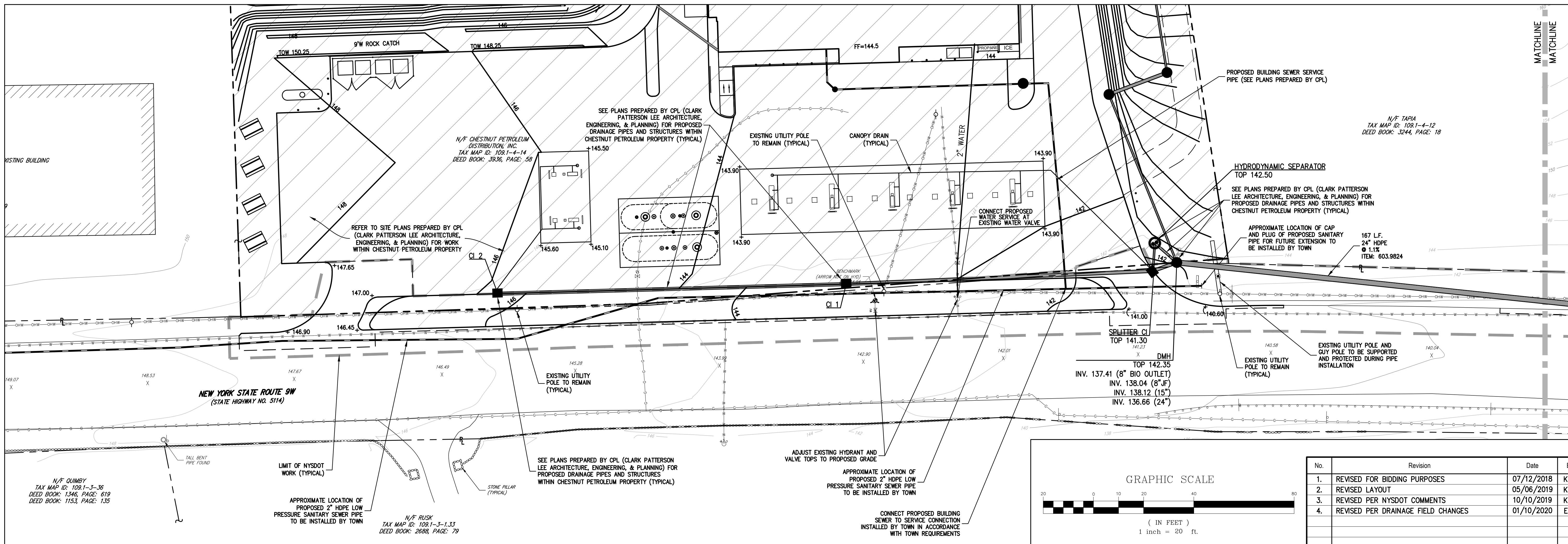
**PROPOSED GAS STATION &
CONVENIENCE STORE**

1417 ROUTE 9W
TOWN OF MARYBROUGH, NEW YORK

<p>APPLICANT/OWNER:</p> <p>CPD ENERGY 536 MAIN STREET NEW PALTZ, NEW YORK 12561</p>	<p>ARCHITECT:</p> <p>CIRO INTERRANTE ARCHITECT, P.C. 85 MARKET STREET BOULICHAZ/ETTERIE, NEW YORK 12601</p>
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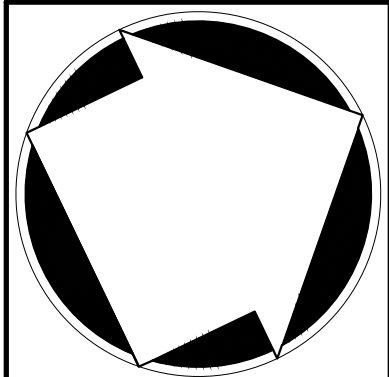
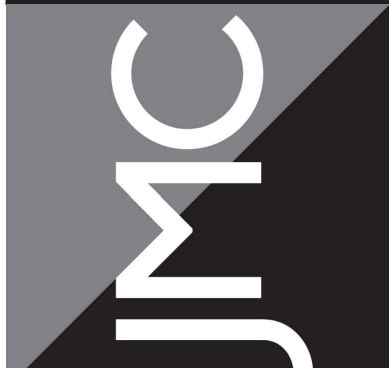
1. EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM SURVEY TITLED, "RIGHT OF WAY & TOPOGRAPHIC SURVEY," PREPARED BY ENGINEERING & SURVEYING PROPERTIES, REVISED 04/18/2018.

- Q1. UNLESS OTHERWISE DIRECTED, CONTRACTOR, AT HIS OWN EXPENSE, WILL PROVIDE A GEOTECHNICAL ENGINEER AS REQUIRED BY THE OWNERS REPRESENTATIVE TO EVALUATE SOIL CONDITIONS RELATED TO TRENCHING, PAVING, SUBGRADE AND OTHER CONDITIONS AS REQUIRED.
- Q2. DURING CONSTRUCTION, PRIOR TO CONTRACTOR PERFORMING ROCK REMOVAL, GEOTECHNICAL ENGINEER SHALL PERFORM SITE INVESTIGATION AND SHALL PROVIDE ROCKOUT/ROCK REMOVAL RECOMMENDATIONS AND PROJECT SPECIFICATIONS TO CONTRACTOR FOR IMPLEMENTATION.
- Q3. CUT AND FILLS SHALL NOT ENDANGER ADJOINING PROPERTIES, NOR DIVERT WATER ONTO THE PROPERTY OF OTHERS.
- Q4. ALL FILLS SHALL BE COMPACTED TO PROVIDE STABILITY OF MATERIAL AND TO PREVENT SETTLEMENT.
- Q5. CONTRACTOR SHALL ADJUST ALL EXISTING STRUCTURES WITHIN THE WORK AREA TO THE PROPOSED GRADE SHOWN ON THE DRAWINGS AS REQUIRED.
- Q6. ALL PROPOSED AND MODIFIED STRUCTURES SHALL BE DESIGNED FOR H=20 LOADING.
- Q7. PROPOSED INLETS & STRUCTURES WITHIN THE NYSDOT RIGHT-OF-WAY SHALL BE MANUFACTURED WITHOUT SUMPS PER NYSDOT STANDARDS.

LEGEND	
	EXISTING PROPERTY LINE
	ADJACENT PROPERTY LINE
	EXISTING BUILDING LINE
	EXISTING PAVEMENT EDGE
	EXISTING CURB LINE
	EXISTING CONTOUR
	EXISTING INDEX CONTOUR
	EXISTING STONE WALL
	EXISTING GUIDE RAIL
	EXISTING DRAIN INLET
	EXISTING MANHOLE
	EXISTING UTILITY POLE
	EXISTING STORM DRAIN LINE AND SIZE
	EXISTING WATER LINE
	EXISTING GAS LINE
	EXISTING OVERHEAD WIRES
	EXISTING DRAIN INLET
	EXISTING MANHOLE
	EXISTING FIRE HYDRANT
	EXISTING GAS VALVE
	EXISTING WATER VALVE
	PROPOSED BUILDING LINE
	PROPOSED CONCRETE CURB /EDGE OF PAVEMENT
	PROPOSED CONCRETE SIDEWALK
	PROPOSED FINISHED GRADE
	PROPOSED SPOT GRADE
	PROPOSED SANITARY SEWER MANHOLE
	PROPOSED STORM DRAIN MANHOLE
	PROPOSED TYPE CI DRAIN INLET
	PROPOSED TYPE DI DRAIN INLET
	PROPOSED STORM DRAIN LINE & SIZE
	PROPOSED RIP-RAP SWALE
	EXISTING FEATURE TO BE REMOVED

APPLICANT/TOWNER: CPD ENERGY 536 MAIN STREET NEW PALTZ, NEW YORK 12561	ARCHITECT: CIRO INTERRANTE ARCHITECT, P.C. 85 MARKET STREET POUGHKEEPSIE, NEW YORK 12601
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**GRADING &
UTILITIES PLAN**

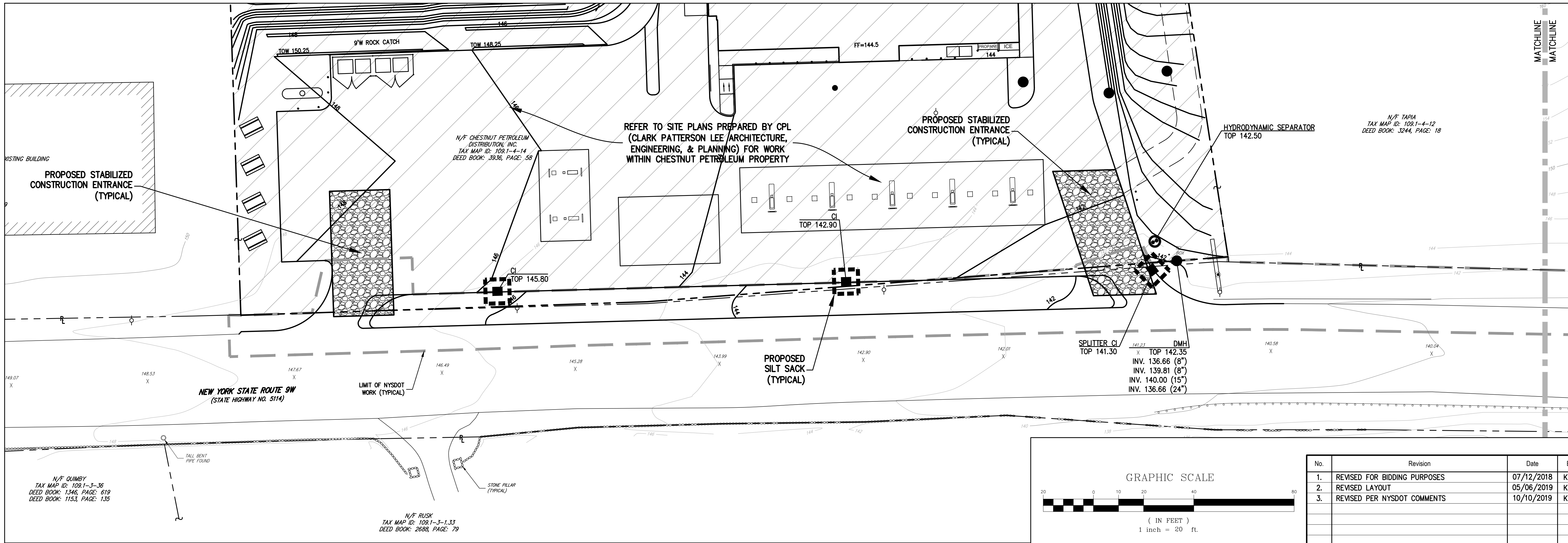
**PROPOSED GAS STATION &
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TOWN OF MARLBOROUGH, NEW YORK

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Drawn:	KRM	Approved:	RJP
Scale:	1" = 20'		
Date:	04/27/2018		
Project No:	15119		
15119-HIGHWAY	GRAD-UTIL	23_HIN-C-User	
Drawing No:			
HP-4			

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No.	Revision	Date	By
1.	REVISED FOR BIDDING PURPOSES	07/12/2018	KRM
2.	REVISED LAYOUT	05/06/2019	KRM
3.	REVISED PER NYSDOT COMMENTS	10/10/2019	KRM
	<i>Previous Editions Obsolete</i>		

Drawn:	KRM	Approved:	RJP
Scale:	1" = 20'		
Date:	04/27/2018		
Project No:	15119		
15119-HIGHWAY	SE	23_HWY-SE.s	
Drawing No:			
HP-5			

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The logo for the Journal of Mathematical Chemistry (JMC) is located in the top right corner. It consists of the letters 'JMC' in a stylized, bold, sans-serif font, with the 'J' and 'M' joined together. Below the text is a circular emblem containing a white pentagon with black triangular segments at its vertices, all set against a black background.

EROSION AND SEDIMENT
CONTROL PLAN

PROPOSED GAS STATION &
CONVENIENCE STORE
1417 ROUTE 9W
TOWN OF MARIBOROUGH NEW YORK

ES01. ALL PROPOSED INLET PROTECTION AND SEDIMENT AND EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL" DATED AUGUST 2005.

ES02. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED PRIOR TO BEGINNING ANY CLEARING, GRUBBING OR EXCAVATION.

ES03. NYSDOT REINFORCED SILT FENCE, ITEM 209.13, TO BE USED IN CONJUNCTION WITH CHECK DAMS (NOT SHOWS) AND TO BE INSTALLED AND MAINTAINED AROUND ALL DISTURBED AREAS, AS DIRECTED BY ENGINEER.

ES04. DISTURBED AREAS IDLE FOR MORE THAN 48 HOURS SHALL BE STABILIZED.

ES05. DURING CONSTRUCTION, ALL EXPOSED AREAS OF SOIL THAT WILL NOT RECEIVE PERMANENT SURFACE TREATMENT IMMEDIATELY (WITHIN SEVEN DAYS) AND ARE NOT PROTECTED BY SILT FENCE SHALL BE PROTECTED BY SILT FENCE IN CONJUNCTION WITH A MIXTURE OF PERENNIAL RYE GRASS, GRASS, ANNUAL RYE GRASS, AND WINTER RYE. GRASS SEED MIX FOR SEDIMENT AND EROSION CONTROL MAY BE USED. OTHER THAN PERENNIAL RYE GRASS, THE DAY AFTER SEEDING, SEEDER AREA SHALL BE MULCHED WITH STRAW AT A RATE OF 2 TONS PER ACRE (90 LBS PER 1,000 S.F.) SUCH THAT THE MULCH FORMS A CONTINUOUS BLANKET.

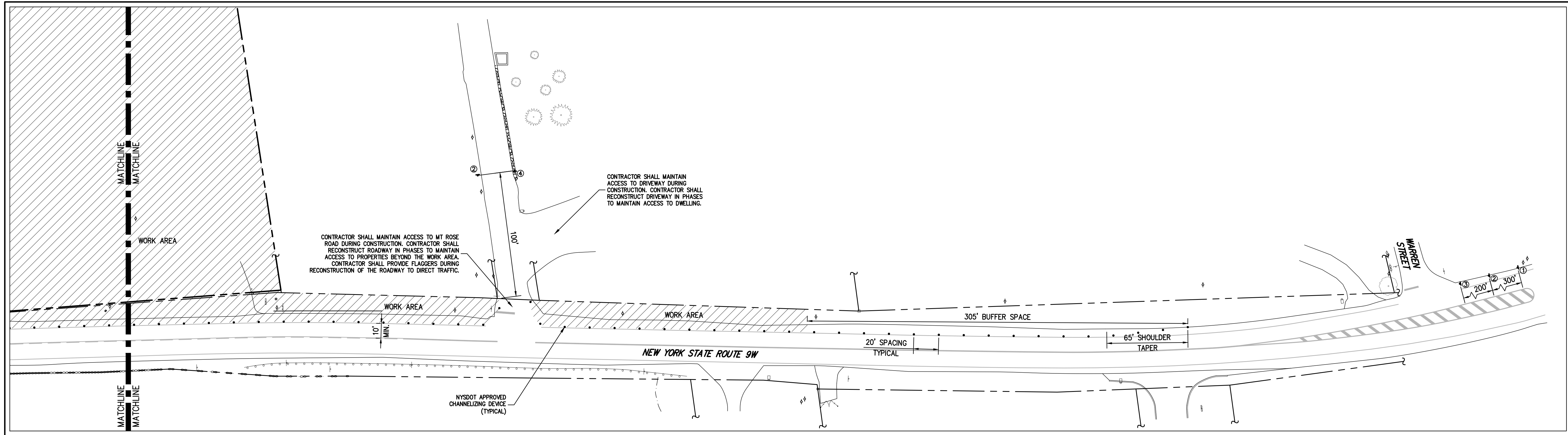
ES06. ALL STOCKPILES OF DIRT SHALL BE RIMMED WITH SILT FENCE BY THE CONTRACTOR IN ADDITION TO BEING TEMPORARILY SEEDED.

ES07. SILT FENCE SHALL REMAIN IN PLACE UNTIL PILE HAS BEEN REMOVED AND SOIL HAS BEEN PERMANENTLY STABILIZED. FOR THOSE AREAS OF EXPOSED SOIL WHERE IT IS NOT FEASIBLE TO ESTABLISH TEMPORARY GROUND COVER DUE TO CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL MOISTEN WITH WATER THOSE AREAS AT LEAST ONCE A DAY. IF DRY, ADDITIONAL SEEDER. DUST CONTRACTOR SHALL INCREASE THE MOISTENING OF SUCH AREAS TO FOUR TIMES A DAY DURING PERIODS OF LITTLE RAIN AS DETERMINED BY THE OWNER'S FIELD REPRESENTATIVE, THE APPROVAL AUTHORITY AND/OR THE CONTRACTOR.

ES08. EXISTING AND PROPOSED CATCH BASINS SHALL BE PROTECTED WITH SILT SACK INLET PROTECTION. INLET PROTECTION SHALL BE INSTALLED BY THE CONTRACTOR AND SHALL BE MAINTAINED DURING CONSTRUCTION. INLET PROTECTION SHALL BE REMOVED ONLY AFTER ALL CONSTRUCTION HAS BEEN COMPLETED AND ALL SOURCES OF EROSION HAVE BEEN PERMANENTLY STABILIZED.

ES09. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR IMMEDIATELY AFTER EACH RAINFALL EVENT AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR IMMEDIATELY AT NO ADDITIONAL COST.

ES010. SEDIMENT DEPOSITS SHALL BE REMOVED BY THE CONTRACTOR WHEN THEY REACH APPROXIMATELY ONE-HALF THE HEIGHT OF THE BARRIER. SEDIMENTS SHALL BE PROPOSED IN A MANNER THAT DOES NOT RESULT IN ADDITIONAL EROSION OR POLLUTION.



W1. ANY ROUTE W/ A STATE ROADWAY, ALL WORK SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS, SPECIFICATIONS, AND CONDITIONS IMPOSED BY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION.

W2. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE FEDERAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, THE NEW YORK STATE SUPPLEMENT, SECTION 619 OF THE N.Y.S.D.O.T. STANDARD SPECIFICATIONS, AND ANY PROVISIONS CONTAINED IN THE PLANS. ANY CHANGES TO THE STANDARD SPECIFICATIONS OR THE MANUAL AND IN THE MUTCD REFLECT THE MINIMUM REQUIREMENTS.

W3. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS. ANY CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN, SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.

W4. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF THE PERSONNEL WHO WILL BE RESPONSIBLE FOR SECURE LANE, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.

W5. THE CONTRACTOR'S OPERATIONS SHALL KEEP INTERFERENCE ON ALL APPROVED TO AN ABSOLUTE MINIMUM. TRAFFIC STOPPAGES WILL NOT BE PERMITTED UNLESS APPROVED IN WRITING BY THE N.Y.S.D.O.T. NO LANE CLOSURE SHALL BE ALLOWED BETWEEN 5:30-9:30 A.M. AND 3:30-7:00 P.M.

W6. THE CONTRACTOR SHALL MAINTAIN A MINIMUM 500 FOOT LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

W7. WHEN TWO OR MORE ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED.

W8. WARNING SHAPED ADVANCED WARNING SIGNS SHALL BE USED FOR ALL ADVANCE WARNING SIGNS THAT MAY BE EITHER DIAMOND OR RECTANGULAR SHAPED ACCORDING TO THE FEDERAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND THE NEW YORK STATE SUPPLEMENT. THE ADVANCE WARNING SIGNS SHALL BE PLACED AT THE SIDE OF THE GROUND AND 2 FEET MINIMUM FROM FACE OF CURB OR EDGE OF TRAVELED WAY. THE STATE ENGINEER AND/OR NYSDOT MAY ORDER ADDITIONAL OR MODIFIED DEVICES AND/OR METHODS TO MEET FIELD CONDITIONS.

W9. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS ARE DEFINITIVE. ANY CHANGES MUST BE APPROVED BY THE ENGINEER AND THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.

- W210. ANY EXISTING SIGNS (NOT SHOW), INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR OTHERWISE APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL LOCATION AND/OR LOCATION UNLESS OTHERWISE SPECIFIED IN THIS CONTRACT.
- W211. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORISTS LINE OF SIGHT.
- W212. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DEPUTY.
- W213. ALL WORK ZONE TRAFFIC CONTROL SIGNAGE AND CONSTRUCTION SIGNAGE SHALL BE CLEAR AND REFLECTIVE.
- W214. THE CERTIFIED FLAGGER SHALL USE STOP-SLOW PADDLES (MINIMUM 24 INCHES IN SIZE) WHERE FEASIBLE. FLAG USE MAY BE STOPPED AT INTERSECTIONS OR WHERE THE BACK SIDE MESSAGE IS INAPPROPRIATE FOR OPPOSING TRAFFIC OR WHERE CONDITIONS SUCH AS HIGH WIND MAKE THE USE OF A PADDLE IMPRACTICAL.
- W215. N.Y.S.D.O.T. DRUMS, IF USED FOR TRAFFIC WARNING, SHALL BE A MINIMUM OF 36 INCHES IN DIAMETER AND HAVE A MINIMUM OF 12 INCHES WIDTH. EACH DRUM SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. THESE WARNINGS ON DRUMS SHALL BE HORIZONTAL, CHROMATIC, ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES 4 TO 6 INCHES WIDE.
- W216. WARNING LIGHTS SHALL BE SECURED TO THE N.Y.S.D.O.T. DRUMS. TYPE A LOW INTENSITY FLASHING LIGHTING SIGNALS SHALL BE MAINTAINED AS TO BE VISIBLE ON A CLEAR NIGHT FROM A DISTANCE OF 3,000 FEET.
- W217. CONTRACTOR SHALL PROVIDE CERTIFIED FLAG PERSONS WITH STOP/SLOW STAFF MOUNTED PADDLE BOARD SIGNS WHEN TRUCKS ENTER AND LEAVE THE SITE.
- W218. CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE ROADSIDE AREA DURING NON-WORKING HOURS.
- W219. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
- W220. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE TIME IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE OF THE CONTRACT LIMITS.
- W221. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2 FOOT LATERAL CLEARANCE TO THE TRAVELED WAY.
- W222. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE 10 FEET.
- W223. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER, A MINIMUM OF 72 HOURS IN ADVANCE, OF ANY PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.

ZM25. BEWARE OF OVERHEAD WIRES.

ZM26. PAVEMENT VEHICLES OWNED BY THE CONTRACTOR OR HIS/HER WORKMEN SHALL NOT BE PARKED ON THE PAVEMENT OR HAZARDOUS, OR ANY OTHER AREAS DEEMED BY THE SITE ENGINEER AND/OR NYSDOT TO BE HAZARDOUS LOCATIONS.

ZM26.10 MATERIAL SHALL BE PLACED ON THE SHOULDER, OR WITHIN 30 FEET FROM THE EDGE OF PAVEMENT, EXCEPT THAT WHICH IS TO BE PLACED THAT DAY.

ZM27. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY FOR MULTIPLE DAYS. ACCESS TO DRIVEWAYS, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.

ZM28. SUTABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

ZM29. WORK ZONE TRAFFIC CONTROL IS THE RESPONSIBILITY OF THE PERMITEE. ANY SUBCONTRACTORS WORKING FOR THE PERMITEE MUST HAVE A COPY OF THE HIGHWAY WORK ZONE MANUAL, 11TH EDITION, AND MUST BE TRAINED IN THE TRAFFIC CONTROL REQUIREMENTS. IT IS STRONGLY ADVISED THAT A "TALGATE" SAFETY MEETING WITH EACH CROWD CREW BE INITIATED BEFORE THE START OF WORK.

ZM30. TRAFFIC CONTROL SCHEMES MUST BE IN PLACE, AND MAINTAINED, THROUGHOUT THE DURATION OF WORK.

ZM31. ANYONE WORKING WITHIN THE HIGHWAY RIGHT-OF-WAY SHALL WEAR HIGH-VISIBILITY APPAREL MEETING THE AASHTO 107-2004 CLASS II STANDARDS AND A HARD HAT.

ZM32. AT THE START OF WORK ON THE PROJECT, ALL WORK ZONE TRAFFIC CONTROL DEVICES SHALL APPEAR IN "ACCEPTABLE" CONDITION. THESE DEVICES SHALL NOT BE ALLOWED TO BECOME "FADING", "WORN", "CRACKED AND WEAK", OR "FADING DUE TO THE USE OF PROJECT FADING" AND DETERIORATED PANELS AND NON-STANDARD LEGENDS ARE NOT ACCEPTABLE.

ZM33. FLAGGER SIGNS ARE TO BE USED ONLY WHEN A FLAGGER IS ACTUALLY PRESENT AND VISIBLE TO THE MOTORIST. THEY SHALL BE COVERED OR REMOVED AT ALL OTHER TIMES. STOP/LOW SLOW PADDLES (MINIMUM 24 INCHES IN SIZE) ARE REQUIRED.

ZM34.10 LONG-MOUNTED SIGNS SHALL BE PERMITTED, EXCEPT FOR FLEXIBLE PANELS MEETING THE CURRENT SPECIFICATION REQUIREMENTS.

ZM35.10 LANE CLOSURES SHALL BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER-IN-CHARGE (EIC).

ZM36.10 LANE CLOSURES WILL BE PERMITTED DURING NON-WORKING HOURS, UNLESS SPECIFICALLY PROVIDED ELSEWHERE IN THE CONTRACT.

ZM37. THE TIME RESTRICTIONS LISTED ABOVE CAN BE SHIFTED AS TRAFFIC CONDITIONS CHANGE. THE ENGINEER-IN-CHARGE SHALL BE NOTIFIED THAT THE ENGINEER-IN-CHARGE (EIC) HAS THE ABILITY TO ORDER ADDITIONAL TIME RESTRICTIONS OF UP TO ONE HOUR PER WORK DAY TO THE TIME RESTRICTIONS LISTED ABOVE FOR TRAFFIC CONDITIONS. HOWEVER, AT NO COST TO THE STATE, THE CONTRACTOR SHALL NOT HAVE ANY DELAY CLAIMS AGAINST THE STATE IF THE EIC ORDERS UP TO ONE HOUR OF ADDITIONAL TIME RESTRICTIONS PER DAY.




23.38. THE ENGINEER-IN-CHARGE IS AUTHORIZED TO REDUCE THE ABOVE TIME RESTRICTIONS AS TRAFFIC CONDITIONS WARRANT.

23.39. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING HOLIDAY WORK RESTRICTIONS ARE APPLICABLE TO THIS PROJECT. DURING THESE HOLIDAY PERIODS, THE CONTRACTOR WILL NOT BE ALLOWED TO PERFORM ANY WORK THAT WILL BE DISRUPTIVE TO TRAFFIC, INCLUDING BUT NOT LIMITED TO LANE CLOSURES. LANE CLOSURES WILL NOT BE PERMITTED DURING THE FOLLOWING STATE RECOGNIZED HOLIDAYS: NEW YEAR'S DAY, MEMORIAL DAY, INDEPENDENCE DAY, LABOR DAY, THANKSGIVING DAY, CHRISTMAS DAY.

IF THE HOLIDAY IS ON A MONDAY, NO LANE CLOSURES WILL BE PERMITTED FROM 12 NOON FRIDAY TO 10 A.M. TUESDAY. IF THE HOLIDAY IS ON A TUESDAY, NO LANE CLOSURES WILL BE PERMITTED FROM 12 NOON FRIDAY TO 10 A.M. WEDNESDAY. IF THE HOLIDAY IS ON A WEDNESDAY, NO LANE CLOSURES WILL BE PERMITTED FROM 12 NOON TUESDAY TO 10 A.M. THURSDAY. IF THE HOLIDAY IS ON A THURSDAY OR FRIDAY, NO LANE CLOSURES WILL BE PERMITTED FROM 12 NOON THE DAY BEFORE THE HOLIDAY TO 10 A.M. MONDAY. IF THE HOLIDAY IS ON A MONDAY, NO LANE CLOSURES WILL BE PERMITTED FROM 12 NOON FRIDAY TO 10 A.M. MONDAY.

23.40. THE CONTRACTOR IS ALSO ADVISED THAT THE STATE RESERVES THE RIGHT TO PRECLUDE LANE CLOSURES DURING PERIODS OF INCLEMENT WEATHER, WET OR IICY PAVEMENT, REDUCED VISIBILITY, TRAFFIC ACCIDENTS OR ANY OTHER EMERGENCIES. THE STATE MAY ALTER ANY LANE CLOSURES SHOULD TRAFFIC CONDITIONS OR OTHER UNFORESEEN CIRCUMSTANCES ARISE WHICH WOULD ADVERSELY AFFECT THE TRAFFIC FLOW. THE CONTRACTOR IS ALSO ALERTED THAT THE STATE MANAGEMENT OF TRAFFIC CONDITIONS MIGHT FORCE HIS/HER CONSTRUCTION OPERATION TO STOP, EVEN DURING TIME WHERE SUCH OPERATION WOULD NORMALLY BE PERMITTED. TEN SUCH OCCURRENCES PER CALENDAR YEAR SHOULD BE TAKEN INTO CONSIDERATION AS A REASONABLE FREQUENCY OF SUCH EVENT WHEN BIDDING THIS PROJECT. THE CONTRACTOR SHALL HAVE NO CLAIM AGAINST THE STATE FOR ANY DELAYS OR EXTRA COSTS INCURRED IN COMING WITH THESE RESTRICTIONS.

THE STATE MAY GRANT A WAIVER OF THESE RESTRICTIONS UPON A TIMELY RECEIPT OF A REQUEST FOR SAID WAIVER FROM THE CONTRACTOR. A MINIMUM OF FIVE WORKING DAYS FOR THE REVIEW OF THE CONTRACTOR'S REQUEST WILL BE REQUIRED.

LEGEND	
	WORK AREA
	TEMPORARY SIGN LOCATION & DESIGNATION
	NYSOT APPROVED CHANNELIZING DEVICE

1. EXISTING CONDITIONS DEPICTED ON THIS PLAN HAVE BEEN TAKEN FROM A SURVEY TITLED, "RIGHT OF WAY & TOPOGRAPHIC SURVEY," PREPARED BY ENGINEERING & SURVEYING PROPERTIES, REVISED 04/18/2018.

[illegible]

**WORK ZONE TRAFFIC
CONTROL PLAN**

**PROPOSED GAS STATION &
CONVENIENCE STORE**
1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK

Drawn:	KRM	Approved:	RJP
Scale:	1" = 40'		
Date:	04/27/2018		
Project No:	15119		
15119-HIGHWAY	WZTC	(_HMY-WZTC.s	
Drawing No:			
HP-6			

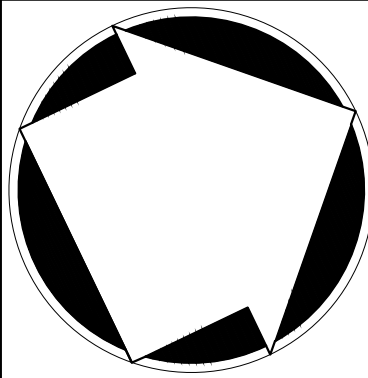
APPLICANT/OWNER: CPD ENERGY
536 MAIN STREET
NEW PALTZ, NEW YORK 12561

ARCHITECT: CIRO INTERRANTE ARCHITECT, P.C.
85 MARKET STREET
POUGHKEEPSIE, NEW YORK 12601

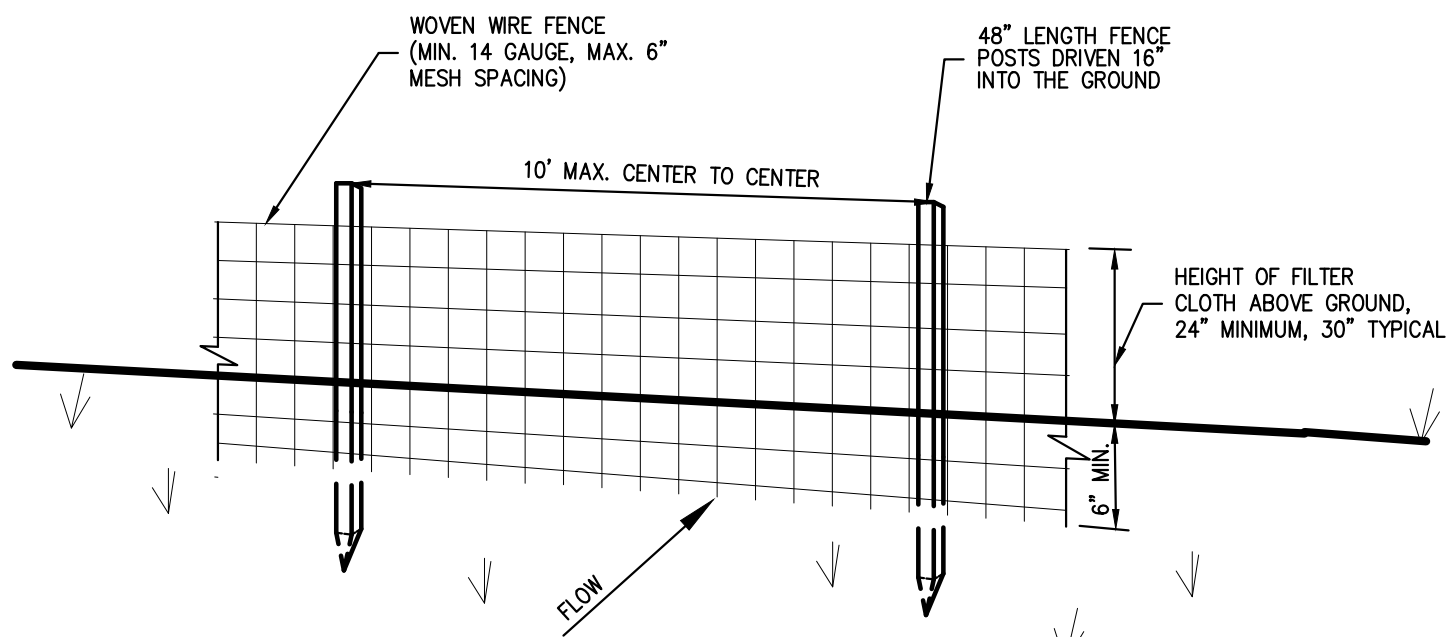
**JMC Planning, Engineering, Landscape
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JMC Site Development Consultants, LLC

John Meyer Consulting, Inc.

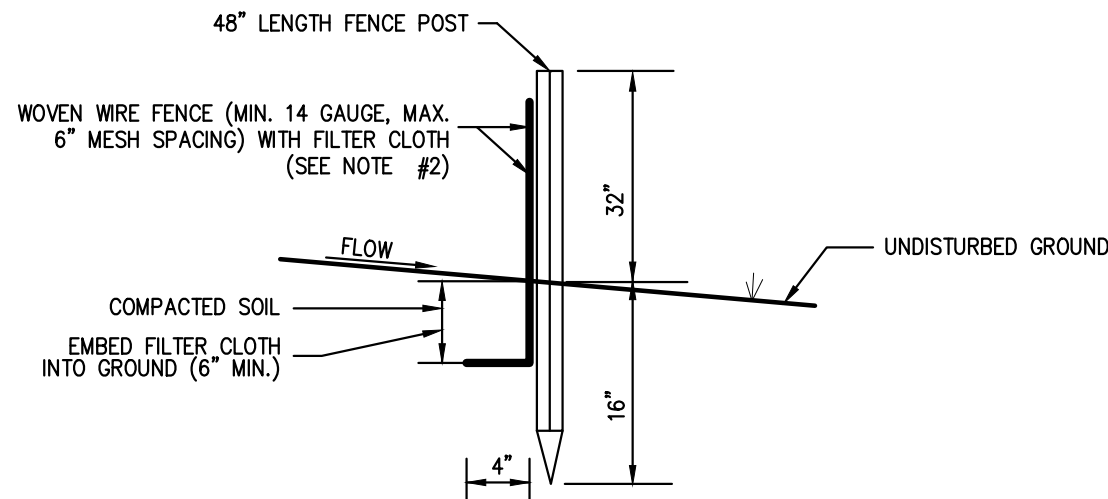
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PERSPECTIVE VIEW



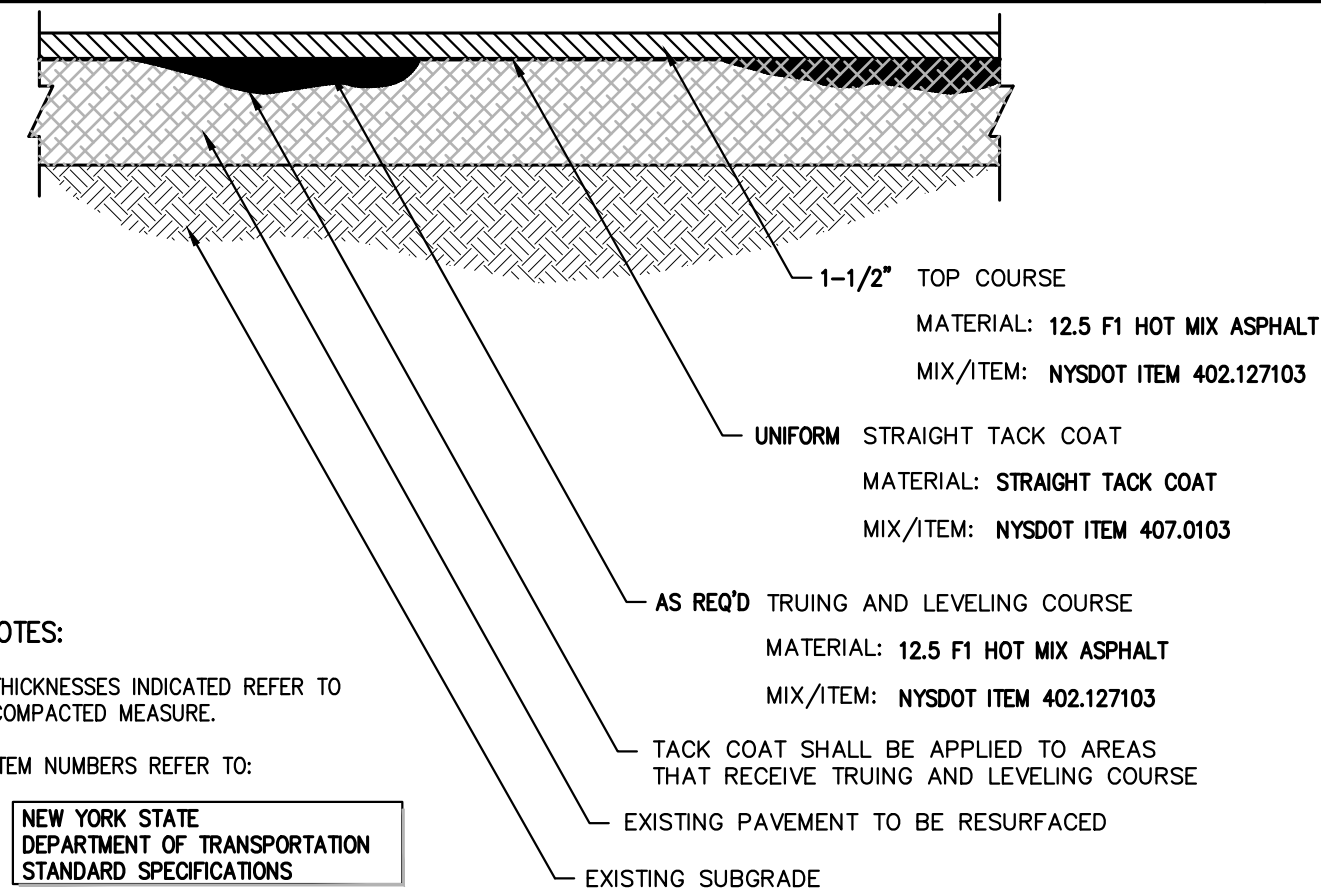
SECTION

NOTES:

- WOVEN WIRE FENCE SHALL BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES. POSTS SHALL BE STEEL, EITHER T OR U TYPE OR HARDWOOD.
- FILTER CLOTH SHALL BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION. FENCE SHALL BE WOVEN WIRE, 6" MAXIMUM MESH OPENING.
- WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY SIX INCHES AND FOLDED. FILTER CLOTH SHALL BE EITHER FILTER X, MIRAFI 100X, STABUNKA 1140N, OR APPROVED EQUAL.
- PREFABRICATED UNITS SHALL BE GEOTAF, ENVIROFENCE, OR APPROVED EQUAL.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN "BULGES" DEVELOP IN THE SILT FENCE.

SILT FENCE

1

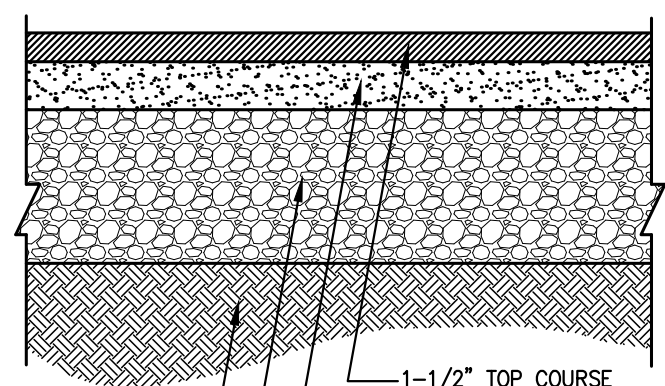


NOTES:

- THICKNESSES INDICATED REFER TO COMPACTED MEASURE.
- ITEM NUMBERS REFER TO:
NEW YORK STATE
DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS

PAVEMENT MILL AND RESURFACING

5



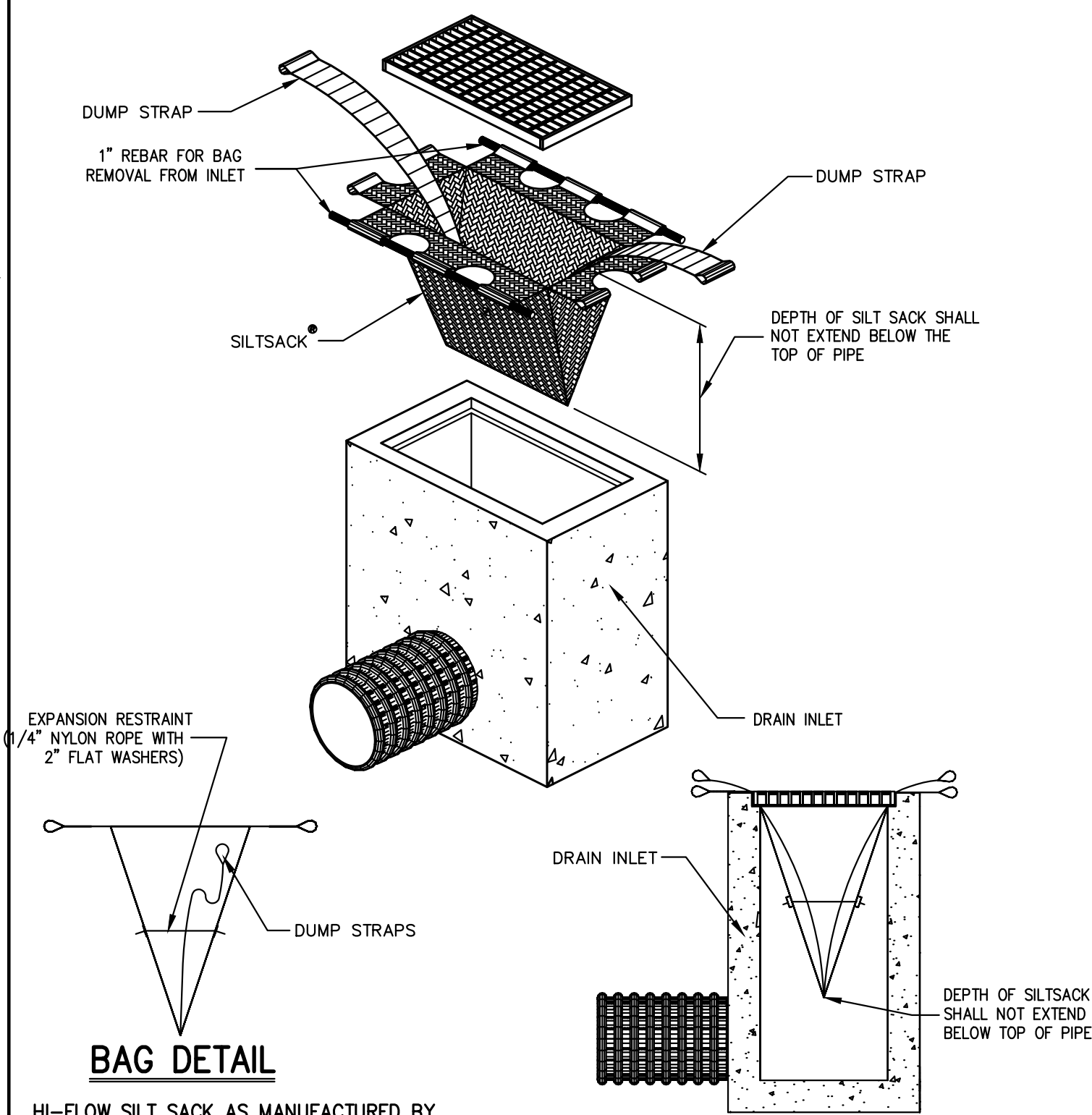
NOTES:

- THICKNESSES INDICATED REFER TO COMPACTED MEASURE.
- MATERIAL AND MIX/ITEM NUMBERS REFER TO:
NEW YORK STATE
DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS
- TACK COATS (ITEM 407.0103) SHALL BE INSTALLED BETWEEN ALL SUPERPAVE COURSES, BETWEEN EXISTING PAVEMENT COURSES & OVERLAYS AND BETWEEN NEW PAVEMENT & EXISTING PAVEMENT EDGES.
- ASPHALT PAVEMENT JOINT ADHESIVE (ITEM 418.7603) SHALL BE INSTALLED AT ALL TOP COURSE PAVEMENT EDGES.

NYSDOT FULL DEPTH PAVEMENT SECTION

(SUPERPAVE)

6



HI-FLOW SILT SACK AS MANUFACTURED BY ACF ENVIRONMENTAL OR APPROVED EQUAL®

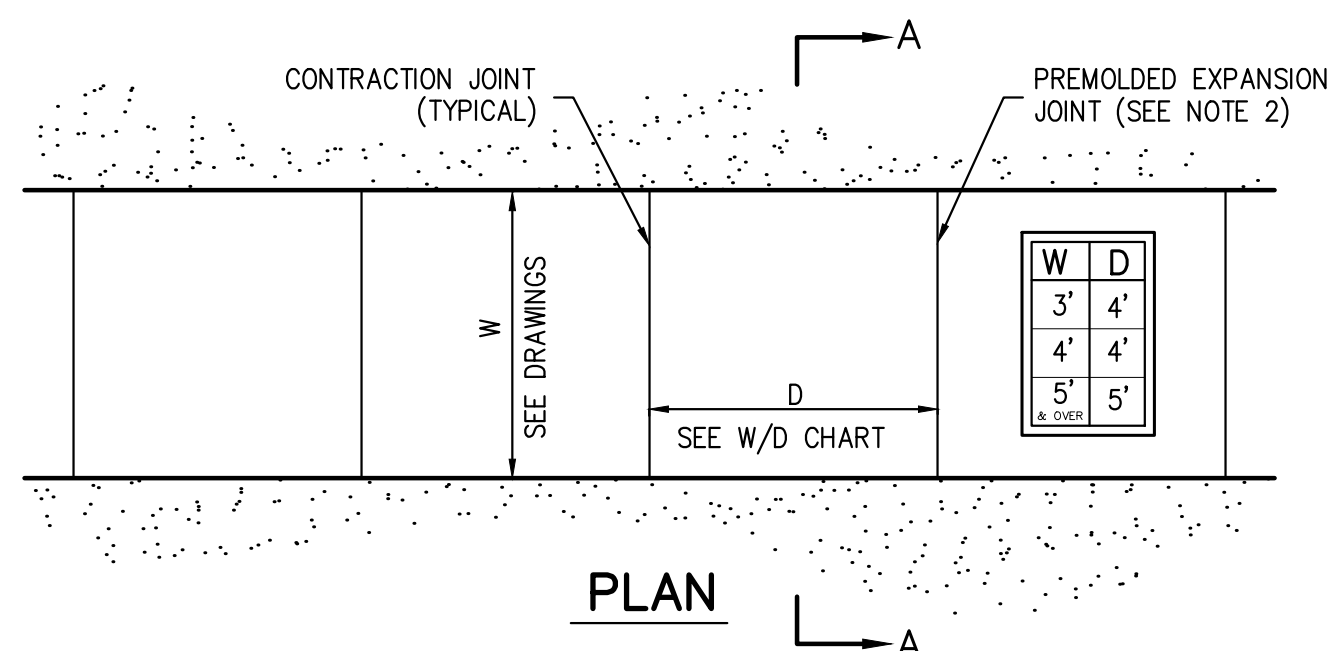
(OR AREAS OF MODERATE TO HEAVY PRECIPITATION AND RUN-OFF)

PROPERTIES	TEST METHOD	UNITS
GRAB TENSILE STRENGTH	ASTM D-4632	255 LBS
GRAB TENSILE ELONGATION	ASTM D-4632	20 %
PUNCTURE	ASTM D-4833	135 LBS
MULLEN BURST	ASTM D-3786	420 PSI
TRAPEZOID TEAR	ASTM D-4533	45 LBS
UV RESISTANCE	ASTM D-4355	90 %
APPARENT OPENING SIZE	ASTM D-4751	20 US SEIVE
FLOW RATE	ASTM D-4491	200 GAL/MIN/SQ FT
PERMITTIVITY	ASTM D-4491	1.5 SEC -1

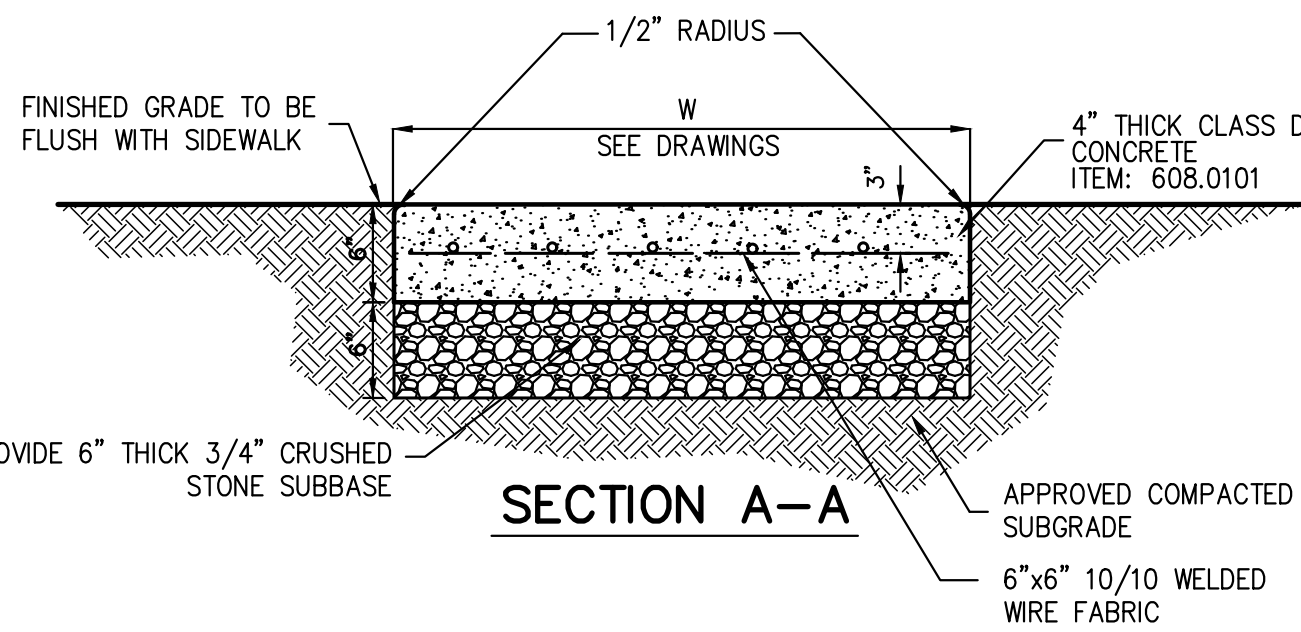
INSTALLATION DETAIL

SILT SACK

2



PLAN



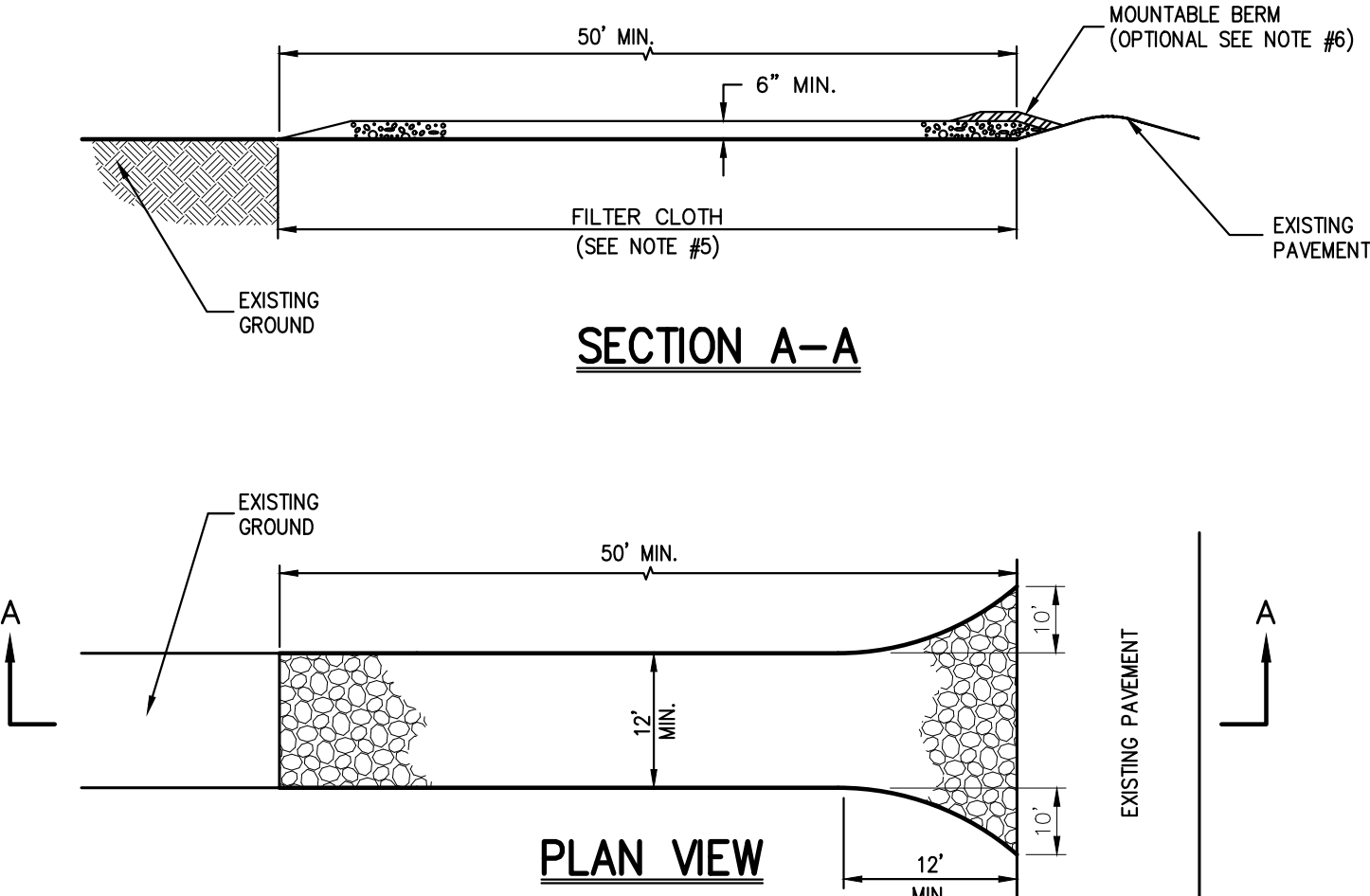
SECTION A-A

NOTES:

- PROVIDE 1/2" PREMOLDED EXPANSION JOINTS AT 20' INTERVALS UNLESS OTHERWISE DIRECTED.
- REINFORCING SHALL NOT EXTEND THROUGH EXPANSION JOINTS.
- ISLAND SHALL HAVE LIGHT BROOM FINISH

CONCRETE ISLAND

7



SECTION A-A

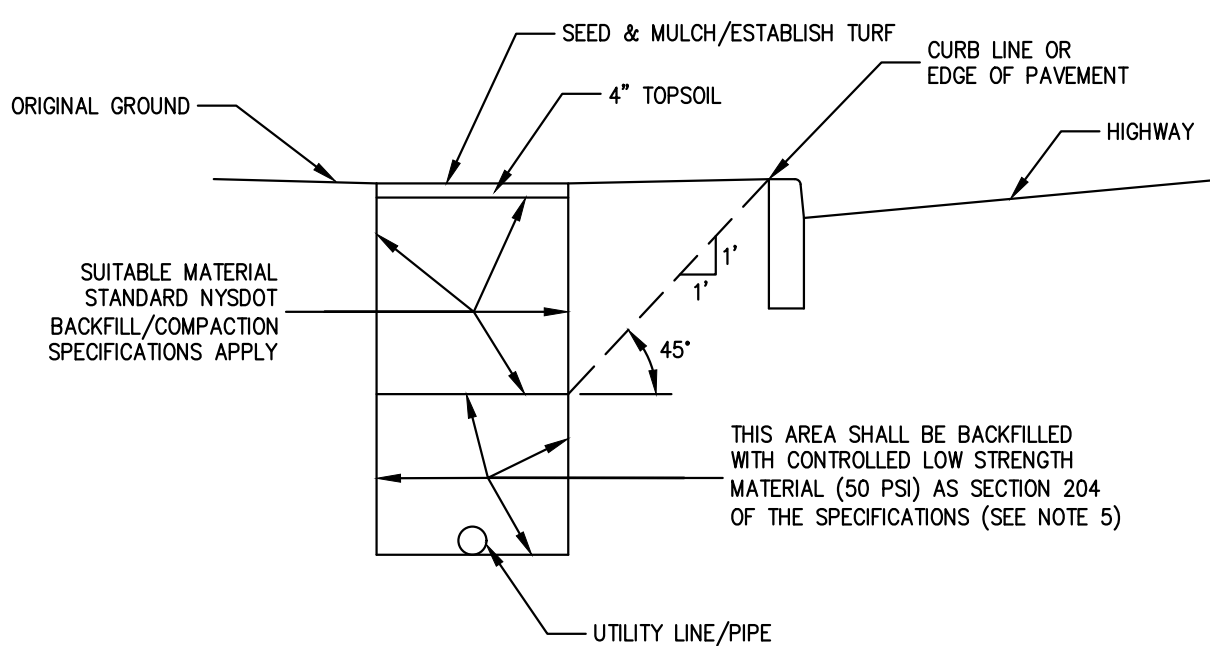
PLAN VIEW

NOTES:

- STONE SIZE - USE 1" TO 4" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- LENGTH - AS REQUIRED, BUT NOT LESS THAN 50 FEET.
- THICKNESS - NOT LESS THAN SIX (6) INCHES.
- WIDTH - 12 FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. 24 FOOT MINIMUM IF SINGLE ENTRANCE TO SITE.
- FILTER CLOTH TO BE PLACED OVER THE ENTIRE WIDTH AND LENGTH OF AREA PRIOR TO PLACING OF STONE.
- SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURE USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

STABILIZED CONSTRUCTION ENTRANCE

3



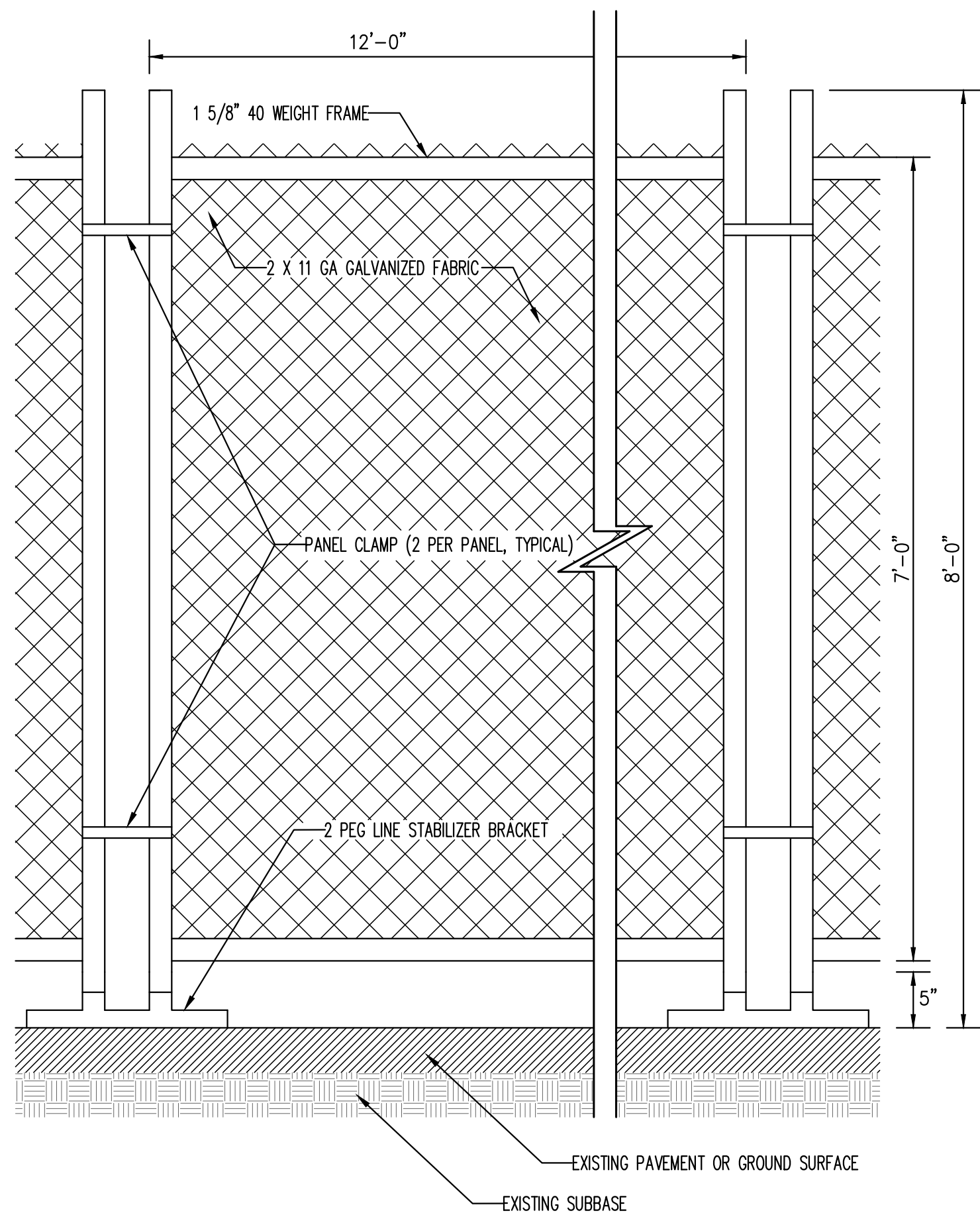
NOTES:

- ALL MATERIALS SHALL BE PROVIDED AND WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION AND STANDARD SPECIFICATIONS AND ALL CURRENT ADDENDUM.
- FOR LIGHTWEIGHT PIPE INSTALLATIONS, IT MAY BE NECESSARY TO COUNTERACT THE BUOYANCY OF THE PIPE DURING THE CLSM PLACEMENT. METHODS SUCH AS PARTIALLY FILLING WATERTIGHT PIPES WITH WATER, OR USING ANCHOR STRAPS OR GRAVEL BAGS TO HOLD DOWN THE PIPES CAN BE USED TO PREVENT FLOTATION.
- DO NOT PLACE CLSM IN CONTACT WITH ALUMINUM PIPE OR CONNECTIONS UNLESS ALUMINUM IS COATED WITH AN APPROVED PRIMER.
- DO NOT PLACE CLSM CONTAINING FLY ASH IN CONTACT WITH CAST IRON OR DUCTILE IRON PIPES OR FITTINGS.
- THE CONTRACTOR MAY SURROUND THE PIPE OR UTILITY WITH SPECIAL MATERIAL SUBJECT TO APPROVAL OF THE ENGINEER.

NYSDOT RESTORATION DETAIL FOR UTILITY LINE EXCAVATION/TRENCH

(EXCAVATION OUTSIDE PAVED SURFACE)

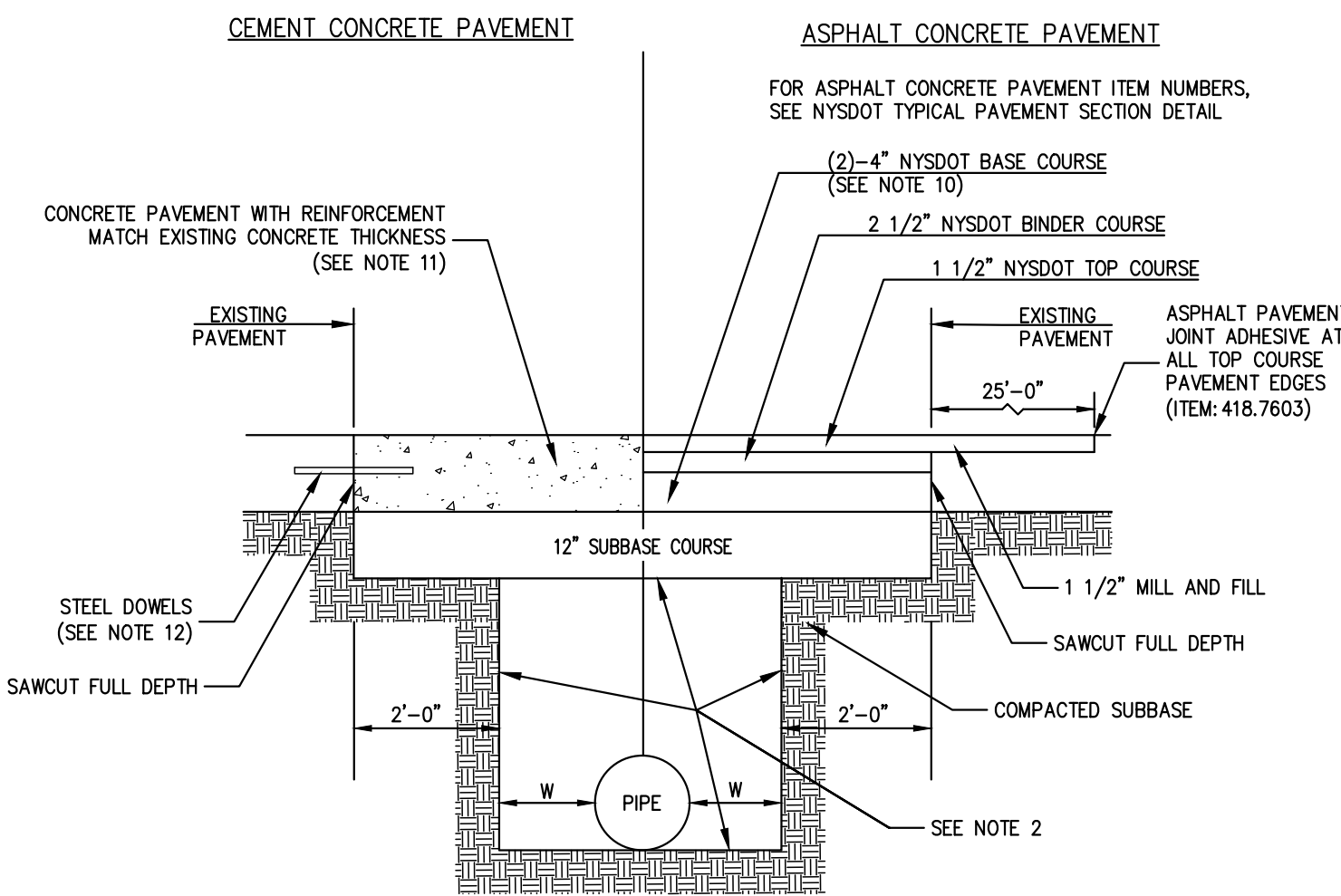
8



NOTE:
STABILIZED BRACKET TO BE HOT DIPPED GALVANIZED STEEL PIPE.

TEMPORARY CHAIN LINK CONSTRUCTION FENCE

4



NOTES:

- ALL MATERIALS SHALL BE PROVIDED AND WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION AND STANDARD SPECIFICATIONS AND ALL CURRENT ADDENDUM.
- WHEN "W" IS LESS THAN 18", CONTROLLED LOW STRENGTH BACKFILL (CLSM) SHALL BE USED. WHEN "W" IS 18" OR GREATER, THE BACKFILL SHALL BE SELECT GRANULAR FILL. MATERIAL AND PLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 200 OF THE STANDARD SPECIFICATIONS. COMPACTION TESTS WILL BE COMPLETED ON IN-PLACE SELECT GRANULAR FILL. THE CLSM HAS TO SET PRIOR TO BACKFILLING. THE CONTRACTOR MAY SURROUND THE PIPE OR UTILITY WITH SPECIAL MATERIAL SUBJECT TO APPROVAL OF THE ENGINEER.
- FOR LIGHTWEIGHT PIPE INSTALLATIONS, IT MAY BE NECESSARY TO COUNTERACT THE BUOYANCY OF THE PIPE DURING THE CLSM PLACEMENT. METHODS SUCH AS PARTIALLY FILLING WATERTIGHT PIPES WITH WATER, OR USING ANCHOR STRAPS OR GRAVEL BAGS TO HOLD DOWN THE PIPES CAN BE USED TO PREVENT FLOTATION.
- WHERE THE DISTANCE BETWEEN THE TOP OF PIPE ELEVATION AND THE TOP OF THE SUBGRADE ELEVATION IS LESS THAN 2', CLSM SHALL BE USED FOR BACKFILL MATERIAL TO TOP OF SUBGRADE ELEVATION.
- WHERE UNSUITABLE MATERIAL IS ENCOUNTERED AT THE BOTTOM OF THE TRENCH, A LAYER OF SELECT GRANULAR FILL OR CLSM SHALL BE PLACED TO PROVIDE A STABLE BEDDING.
- DO NOT PLACE CLSM IN CONTACT WITH ALUMINUM PIPE OR CONNECTIONS UNLESS ALUMINUM IS COATED WITH AN APPROVED PRIMER.
- DO NOT PLACE CLSM CONTAINING FLY ASH IN CONTACT WITH CAST IRON OR DUCTILE IRON PIPES OR FITTINGS.
- ALL PAVEMENT JOINTS SHALL BE SEALED.
- ALL DISTURBED OR NEW PAVEMENT SHALL HAVE TEMPORARY PAVEMENT STRIPING INSTALLED WITHIN 24 HOURS, AND FINAL STRIPING BEFORE ACCEPTANCE OF WORK.
- THE 8" ASPHALT PAVEMENT BASE COURSES SHALL BE PLACED IN LIFTS NO GREATER THAN 4". IF THE EXISTING PAVEMENT IS GREATER THAN 12" TOTAL THICKNESS, THE BASE COURSE THICKNESS SHALL BE INCREASED SO THE BOTTOM OF THE RESTORATION PAVEMENT MATCHES THE EXISTING PAVEMENT.
- THE CEMENT CONCRETE PAVEMENT SHALL MEET THE REQUIREMENTS OF SECTION 502 OF THE SPECIFICATIONS. THE THICKNESS SHALL MATCH THE EXISTING CONCRETE PAVEMENT. THE STEEL REINFORCEMENT SHALL BE #4 BARS @ 12" CENTERS IN BOTH DIRECTIONS AND MEET REQUIREMENTS OF SECTION 556 OF THE SPECIFICATIONS. THE BARS SHALL HAVE 4" COVER AND BE PLACED NO LOWER THAN THE MIDPOINT OF THE SLAB.
- DRILL AND GROUT #8 BARS INTO EXISTING CONCRETE SLAB 12" MINIMUM @ 18" ON CENTER. BARS SHALL BE A MINIMUM OF 24" LONG.
- WHEN CEMENT CONCRETE PAVEMENT HAVING AN ASPHALT OVERLAY IS ENCOUNTERED, PAVEMENT REPLACEMENT OVER TRENCH SHALL FOLLOW THE REQUIREMENTS FOR ASPHALT CONCRETE PAVEMENT REPLACEMENT.

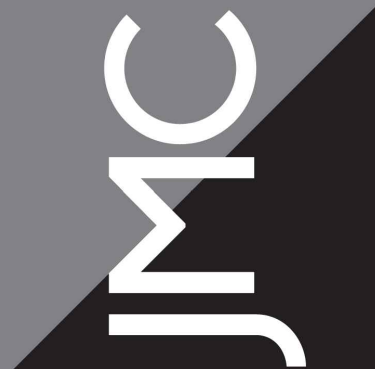
NYSDOT RESTORATION DETAIL FOR UTILITY LINE EXCAVATION/TRENCH

(EXCAVATION WITHIN PAVED SURFACE)

9

No.	Revision	Date
1.	REVISED FOR BIDDING PURPOSES	07/12/2018
2.	REVISED LAYOUT	05/06/2019
3.	REVISED PER NYSDOT COMMENTS	10/10/2019

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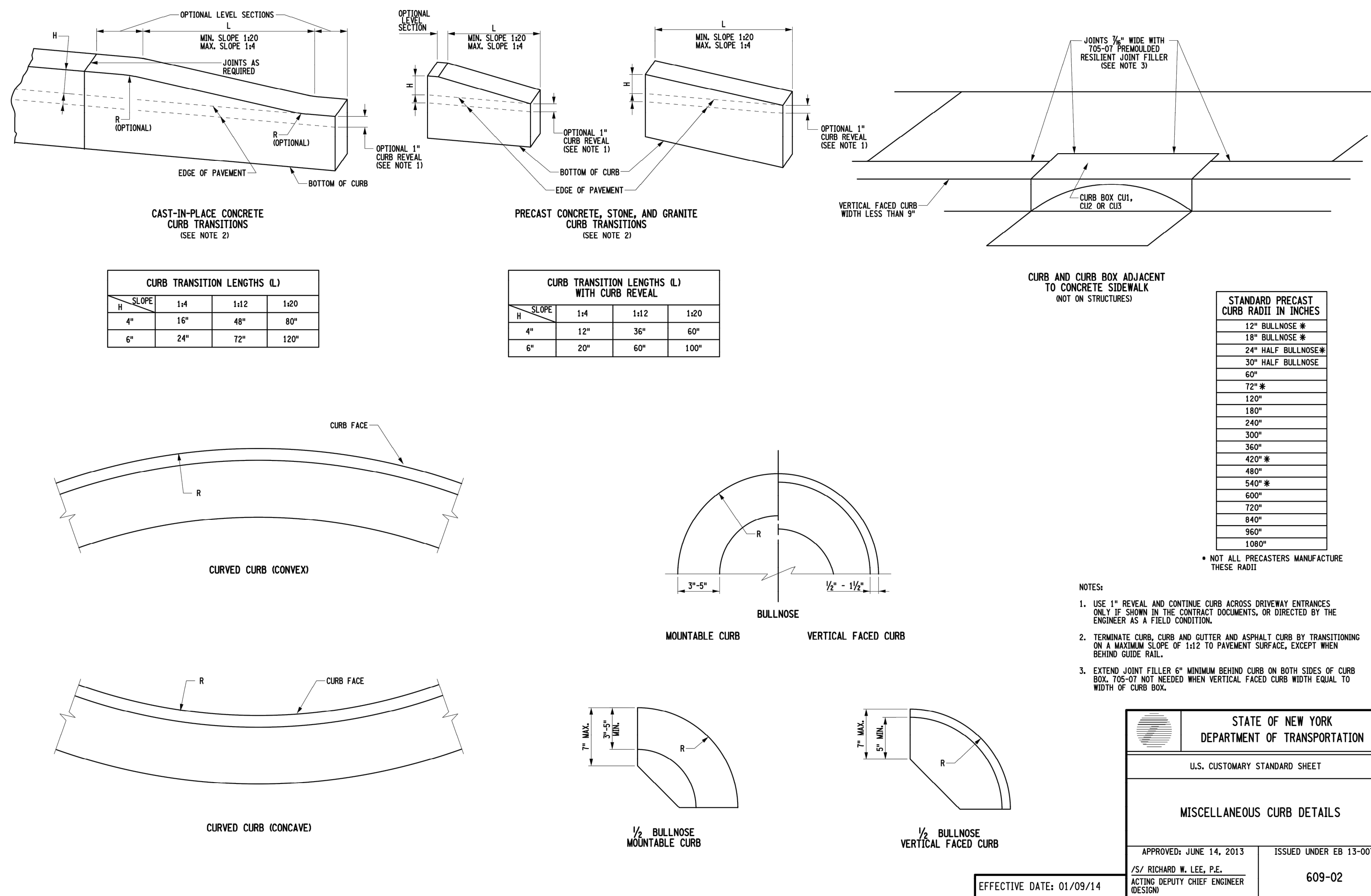
NYSDOT DETAILS

PROPOSED GAS STATION & CONVENIENCE STORE

1417 ROUTE 9W
TOWN OF MARLBOROUGH, NEW YORK

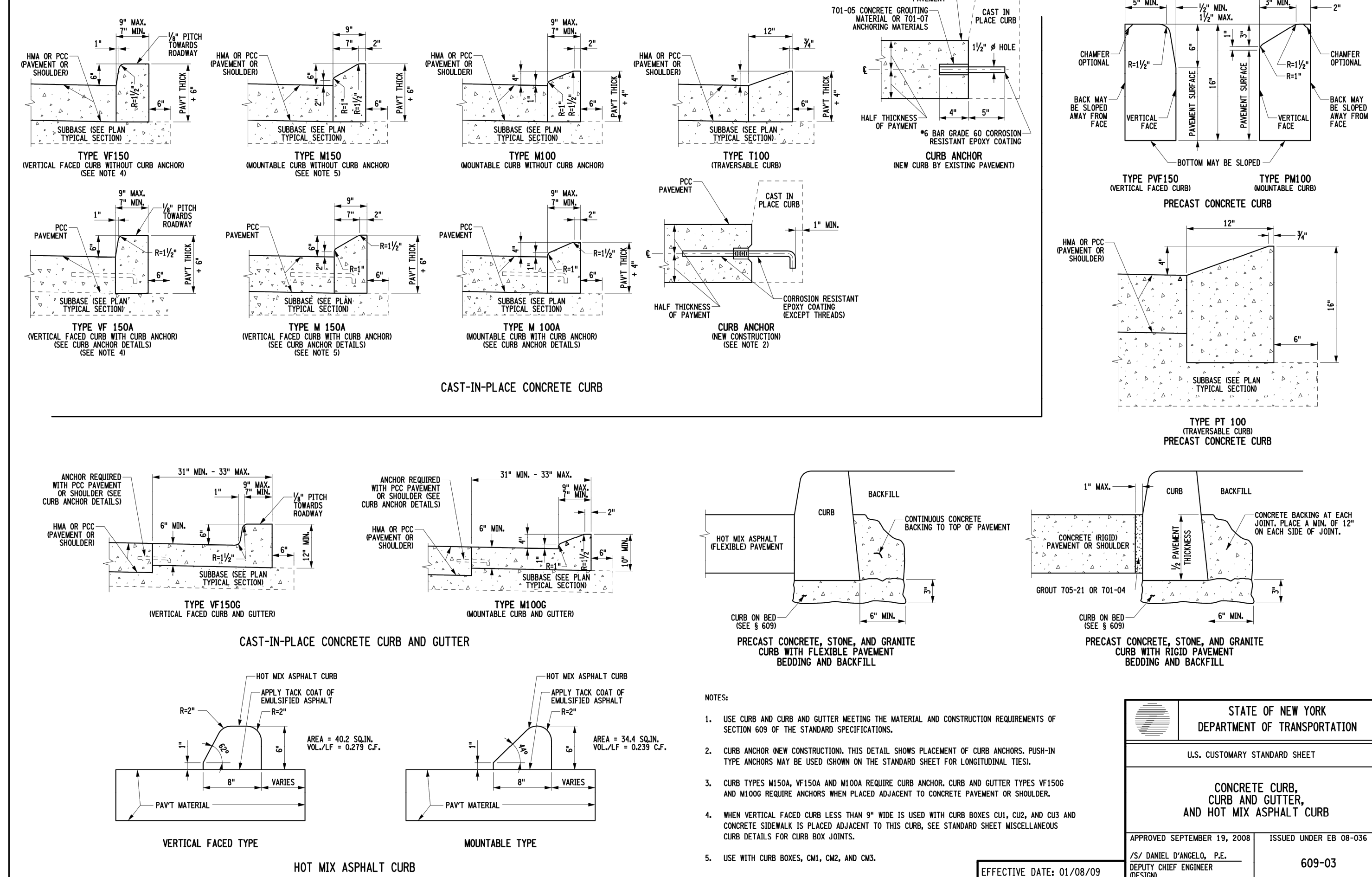
ANY ALTERATION OF PLANS, SPECIFICATIONS, PLATS AND REPORTS BEARING THE SEAL OF A LICENSED PROFESSIONAL ENGINEER OR LICENSED LAND SURVEYOR IS A VIOLATION OF SECTION 7209 OF THE NEW YORK STATE EDUCATION LAW, EXCEPT AS PROVIDED FOR BY SECTION 7209, SUBSECTION 2.

Drawn:	KRM	Approved:	RJP
Scale:	NOT TO SCALE		
Date:	04/27/2018		
Project No:	15119		
1519-ITEMS	BET-1		-
Drawing No:	HP-7		



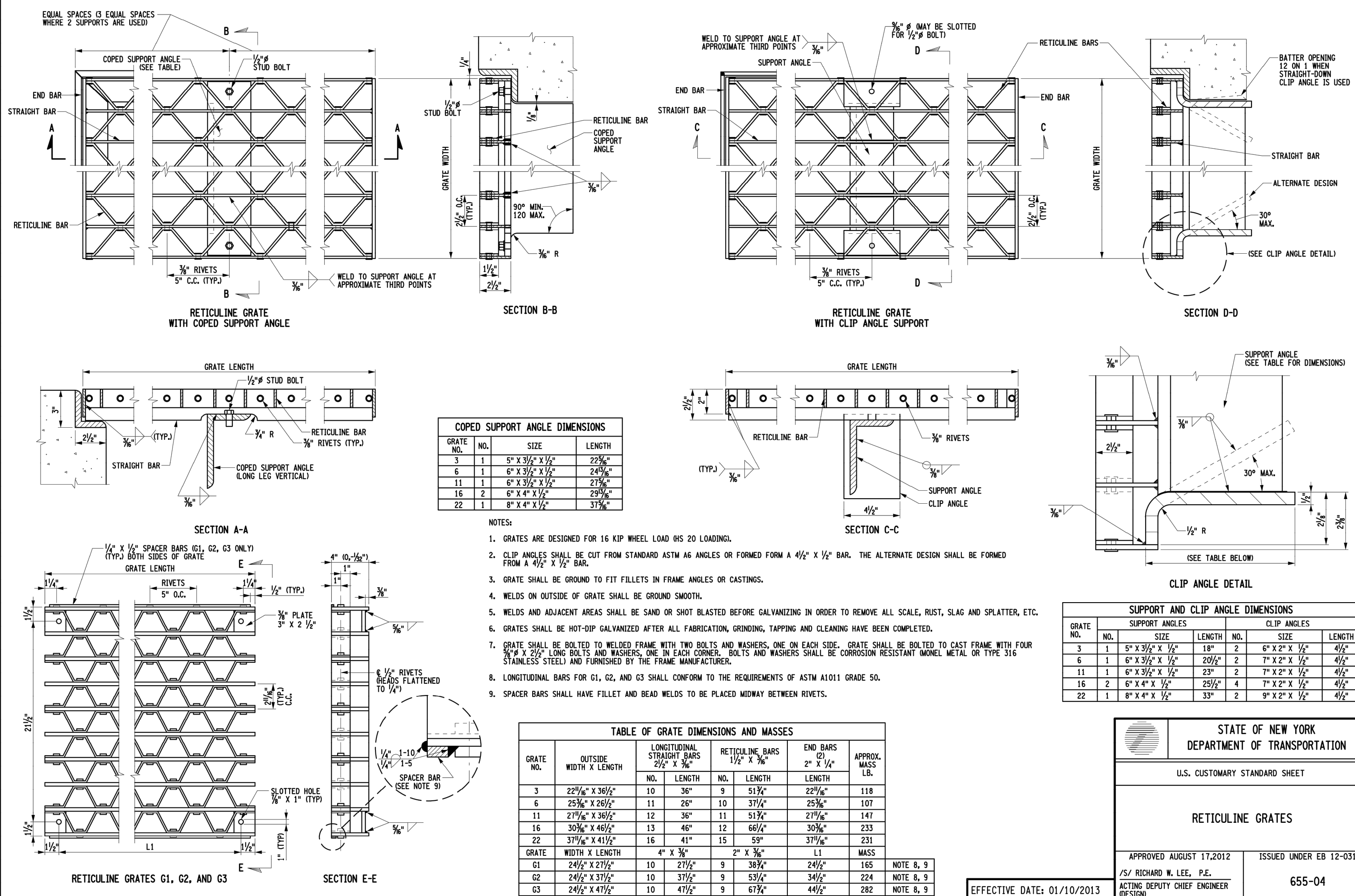
MISCELLANEOUS CURB

10



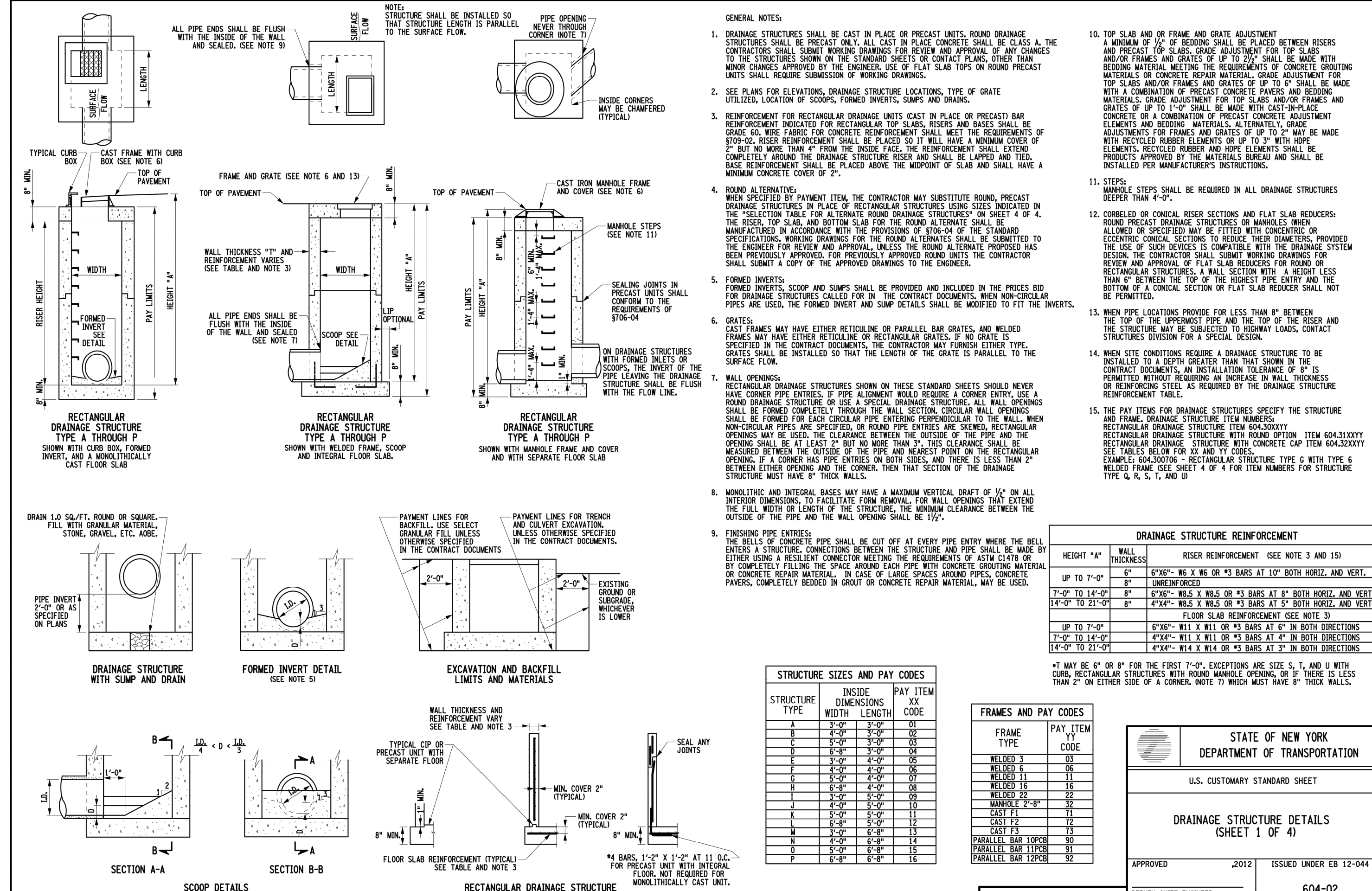
CONCRETE CURB, CURB AND GUTTER, AND HOT MIX ASPHALT CURB

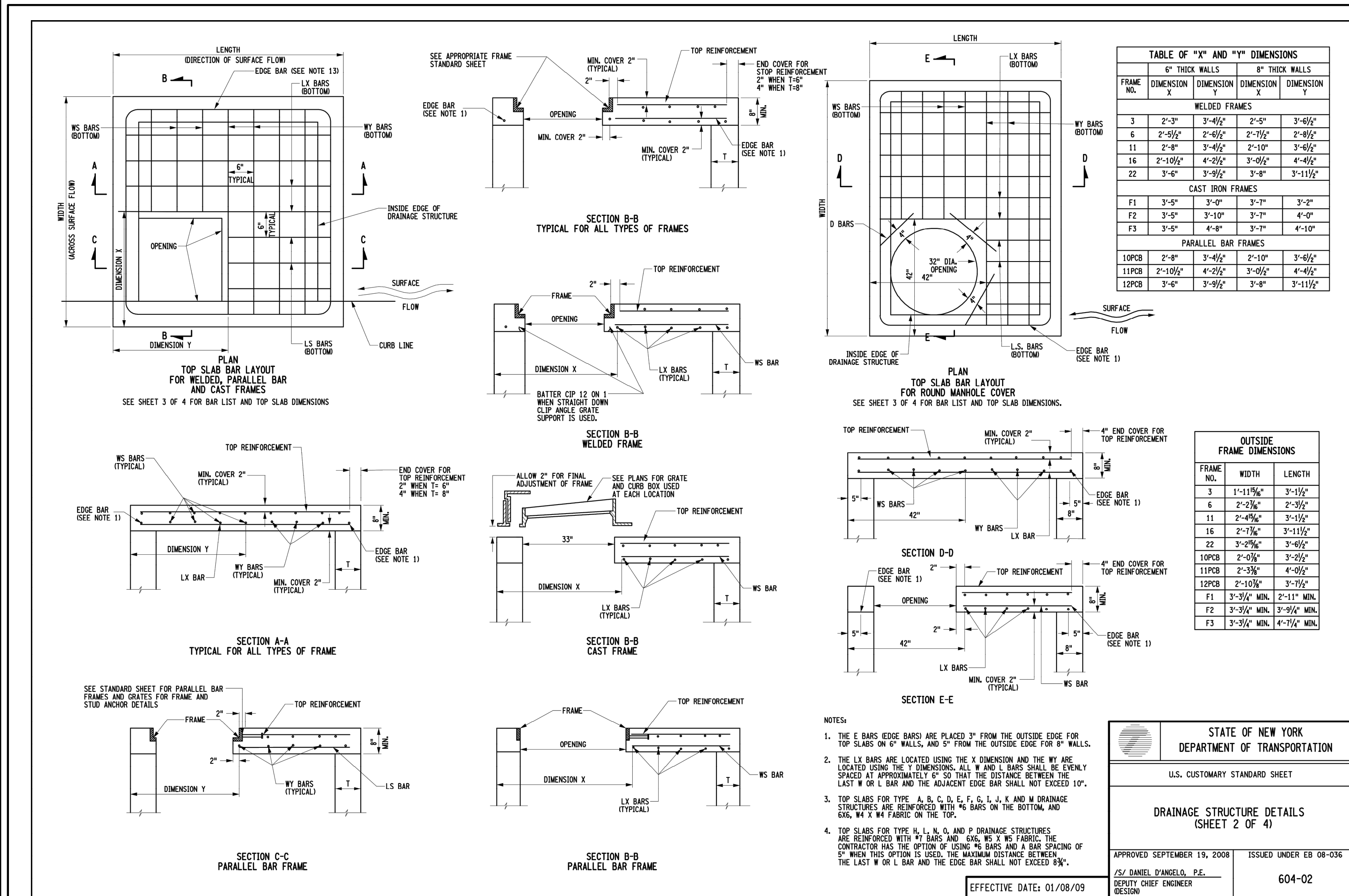
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NYSDOT RETICULINE GRATES

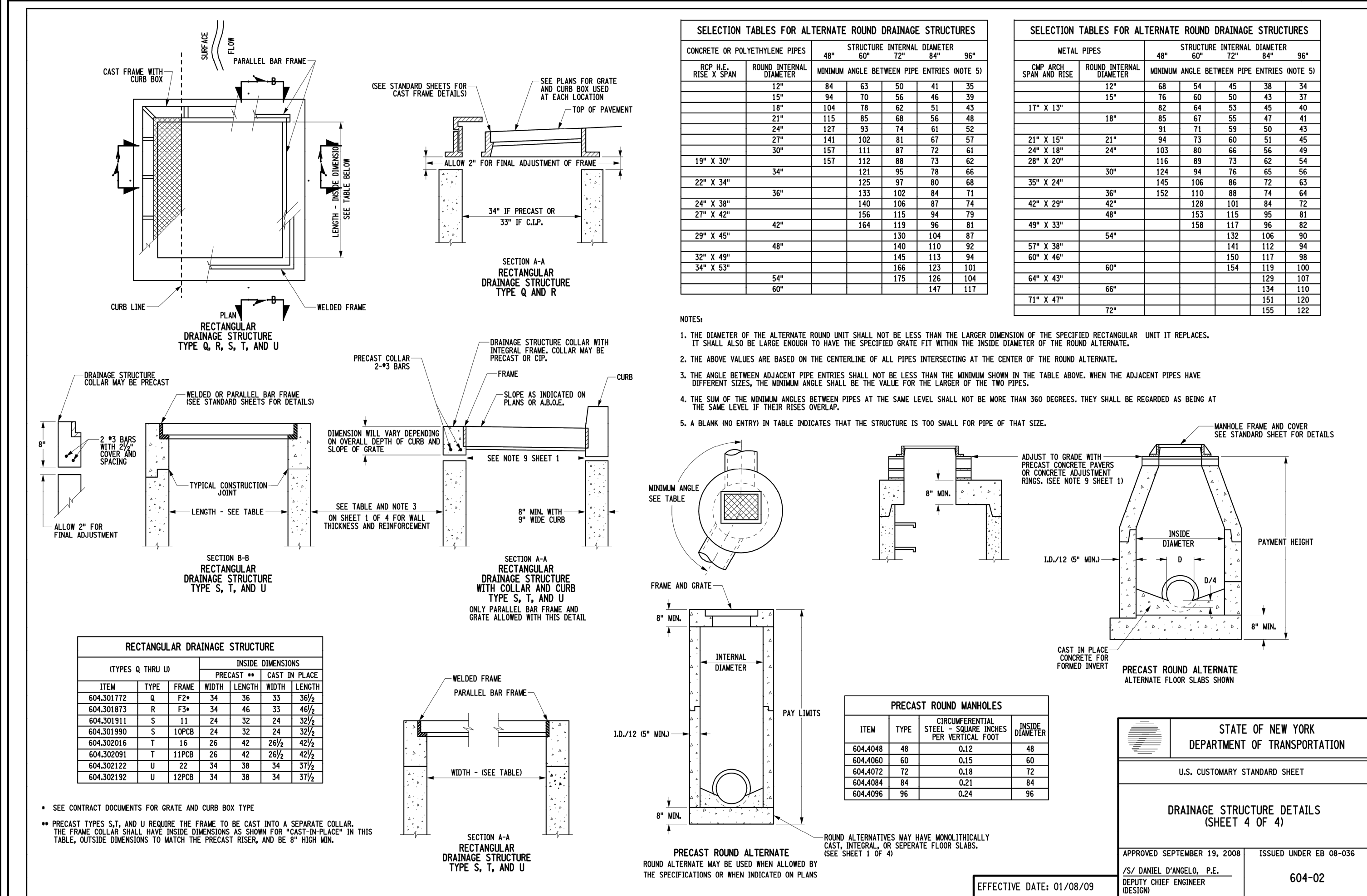
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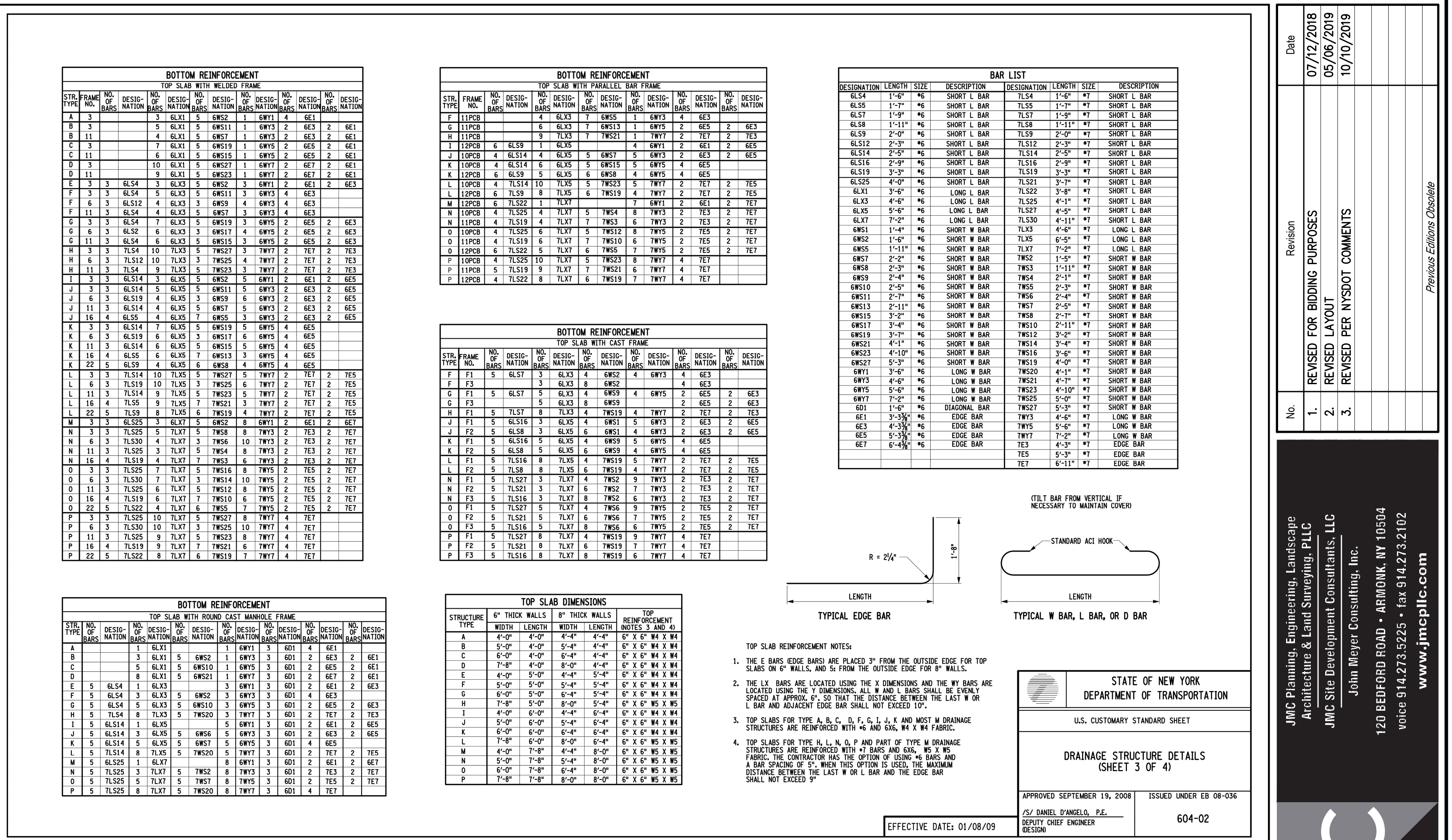
NYSOT DRAINAGE STRUCTURE

14



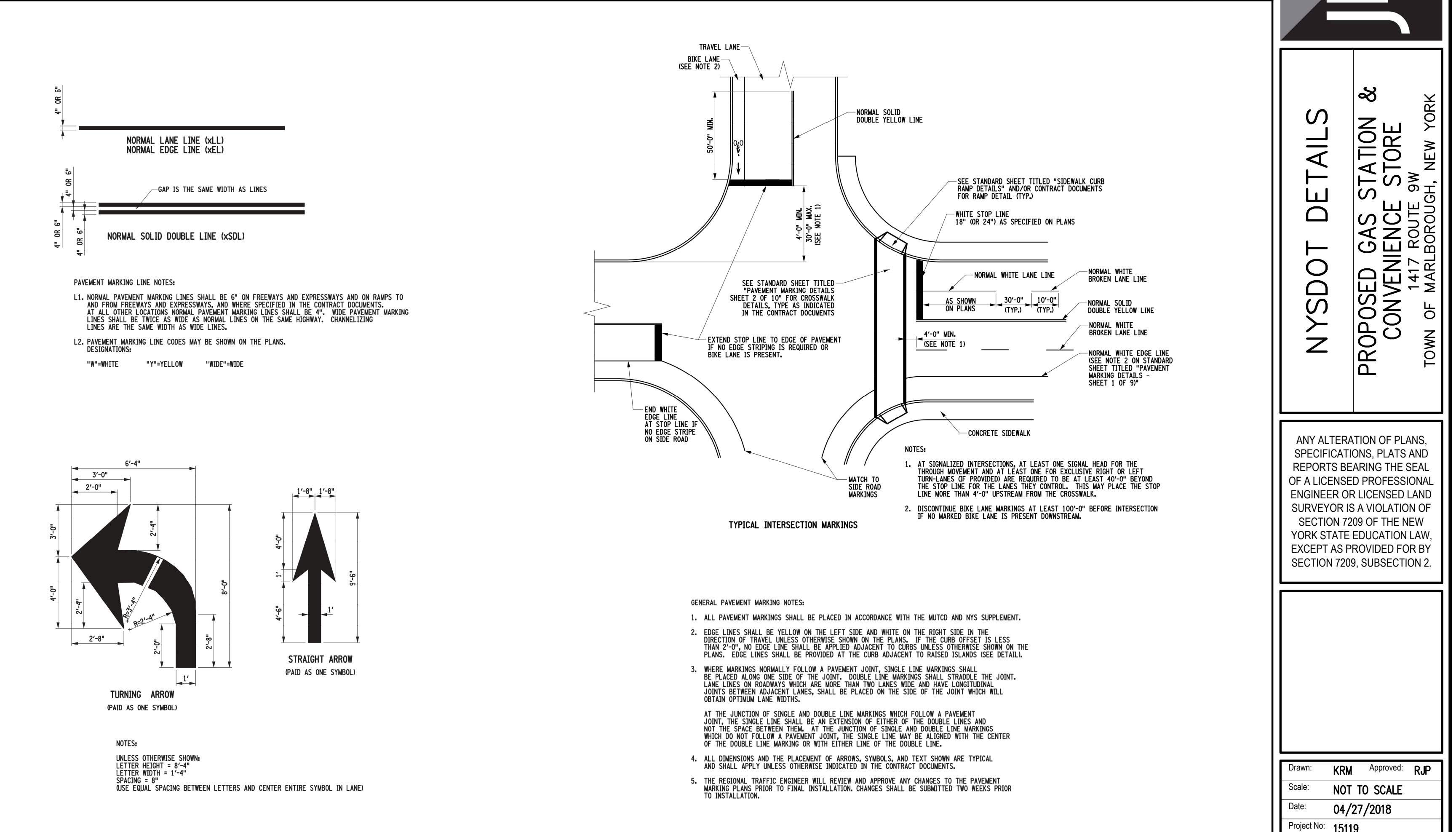
NYSOT DRAINAGE STRUCTURE

16



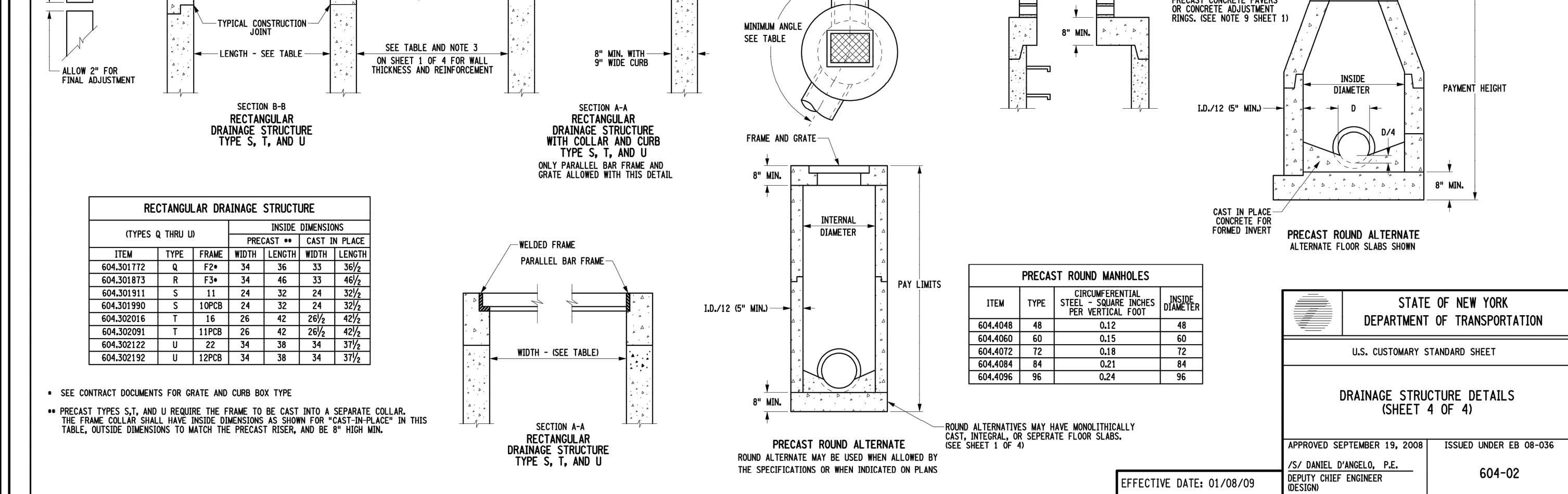
NYSOT DRAINAGE STRUCTURE

15



NYSOT DRAINAGE STRUCTURE

17



NYSOT DRAINAGE STRUCTURE

17

NYSOT DRAINAGE STRUCTURE

16

NYSOT DRAINAGE STRUCTURE

17

NYSOT DRAINAGE STRUCTURE

16

NYSOT DRAINAGE STRUCTURE

17

NYSOT DRAINAGE STRUCTURE

16

NYSOT DRAINAGE STRUCTURE

17

TABLE 1: FLAT-BACK POST PROPERTIES & DIMENSIONS									
POST SIZE	AREA (in ²)	I (in ⁴)	S (in ³)	A	B	C	D	E	MOMENT*
2.00 B/FT	0.55	0.16	0.19	1.503	3.125	1.250	0.114	0.116	931 FF-Ba
2.50 B/FT	0.69	0.21	0.26	1.549	3.125	1.250	0.160	0.130	1274 FF-Ba
3.00 B/FT	0.86	0.24	0.36	1.750	3.500	1.625	0.160	0.150	1764 FF-Ba
4.00 B/FT	1.15	0.45	0.47	1.810	3.500	1.625	0.260	0.182	2303 FF-Ba

* REDUCE MOMENT CAPACITY BY 25% TO ACCOUNT FOR TORSION EFFECTS IN OPEN CROSS-SECTION SINGLE POST INSTALLATIONS.

TABLE 2: INDENTED-BACK POST PROPERTIES & DIMENSIONS									
POST SIZE	AREA (in ²)	I (in ⁴)	S (in ³)	A	B	C	D	E	MOMENT*
2.00 B/FT	0.60	0.19	0.24	1.562	3.083	1.211	0.113	0.112	931 FF-Bb
2.50 B/FT	0.74	0.25	0.30	1.607	3.083	1.211	0.155	0.121	1274 FF-Bb
3.00 B/FT	0.90	0.38	0.45	1.825	3.458	1.577	0.157	0.140	1764 FF-Bb
4.00 B/FT	1.16	0.53	0.59	1.895	3.551	1.577	0.225	0.154	2303 FF-Bb

* REDUCE MOMENT CAPACITY BY 25% TO ACCOUNT FOR TORSION EFFECTS IN OPEN CROSS-SECTION SINGLE POST INSTALLATIONS.

TABLE 3: MAXIMUM NUMBER OF POSTS WITHIN 7 FT.SWATH						
EMBEDMENT TYPE	SOIL SLOPE	S - 1 NON-PLASTIC (MED TO VERY COMPACT)		S - 2 NON-PLASTIC (LOOSE TO VERY LOOSE)		SOIL TYPE
		MAX NO. OF POSTS	MIN. EMBEDMENT	MAX NO. OF POSTS	MIN. EMBEDMENT	
EZE-ERECT	FLAT	2	38"	NO	1	38"
	MODERATE	2	54"	NO	1	54"
	STEEP	2	60"	NO	1	60"
BASE-BOLTED	FLAT	3	38"	NO	3	60"
	MODERATE	3	54"	NO	3	60"
	STEEP	3	60"	NO	3	60"

POSTS SHALL BE PRODUCED FROM STANDARD 1" RAIL STEEL WEIGHING 91 LB/YD OR MORE ACCORDING TO ASTM A-595, GRADE 50, HAVING A TO 1% MINIMUM YIELD (SEE TABLES 1 & 2). POST WEIGHT IS FROM 2.00 TO 4.00 LBS/FT BEFORE PUNCHING.

SOIL POSTS SHALL BE PUNCHED THEIR FULL LENGTH WITH 3/8" # HOLES ON 1" CENTERS.

BASE POSTS SHALL BE PUNCHED WITH A MINIMUM OF TWENTY-FOUR 3/8" # HOLES ON 1" CENTERS (TO ACCOMMODATE OPTIONAL SOIL PLATE) WITH THE FIRST HOLE 1" FROM THE TOP. THE FIRST AND THE FIFTH HOLES SHALL BE 3/8" X 0.000" SLOTS TO FACILITATE CORRECT FIELD ALIGNMENT.

BASE POSTS SHALL BE GIVEN TO 38" MINIMUM EMBEDMENT (SEE TABLE 3) A MAXIMUM OF 4" MAY PROTRUDE ABOVE THE SURROUNDING GROUND SURFACE.

POSTS SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A-123.

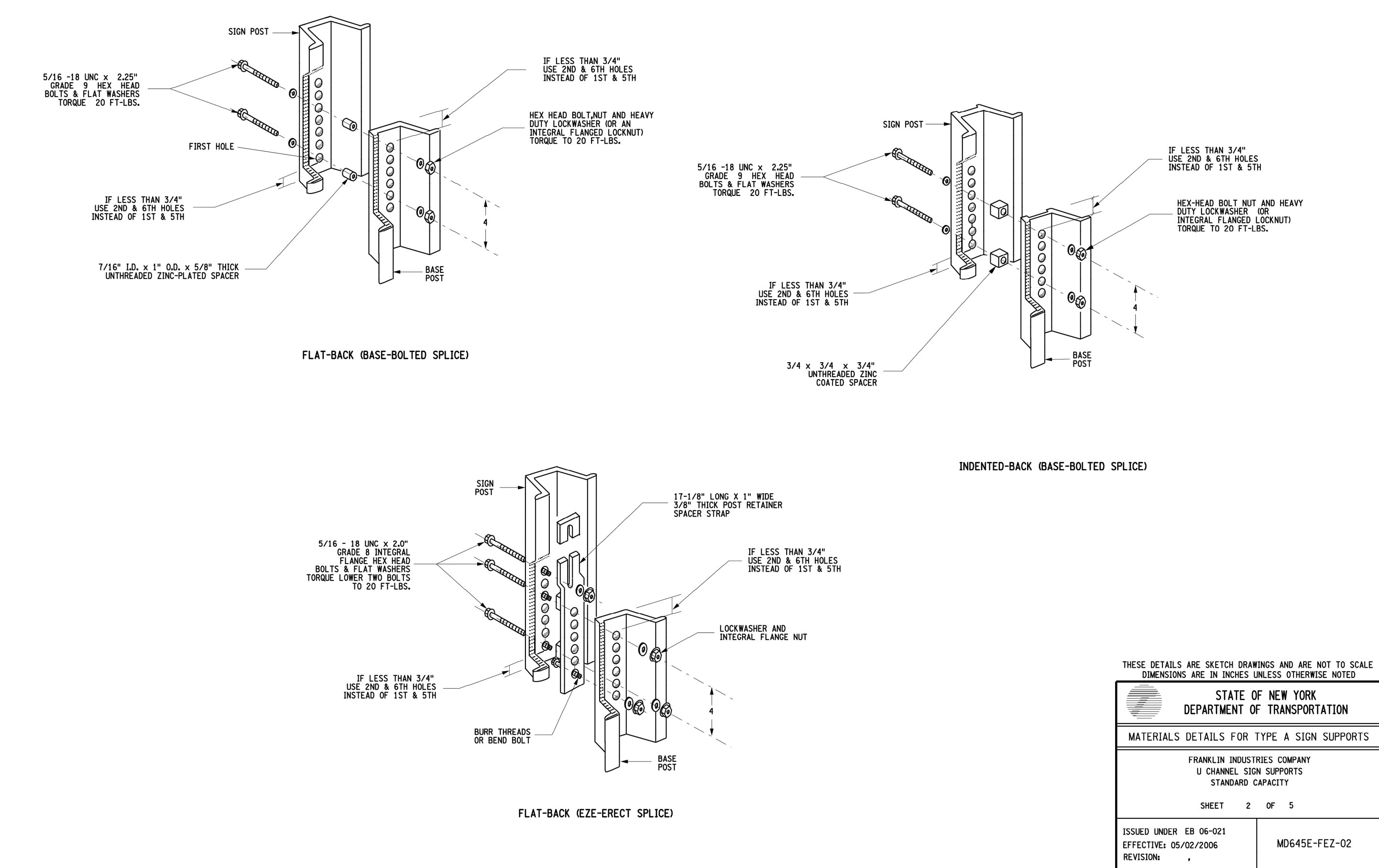
SPLICE HARDWARE SHALL BE AS SUPPLIED OR RECOMMENDED BY THE MANUFACTURER, CADMIUM PLATED ACCORDING TO ASTM A-165, TYPE 05, EXCEPT USING CLEAR CHROMATE.

ALL GALVANIZING AND GALVANIZING REPAIRS SHALL BE DONE IN ACCORDANCE WITH 8719-01,TYPE III.

AN S-1 SOIL CONDITION SHOULD BE ASSUMED. S-1 INCLUDES SANDS, GRAVELS AND SILTS AND THEIR MIXTURES WHICH EXHIBIT PENETRATION VALUES OF 9 OR MORE BLOWS PER FOOT USING A STANDARD 300 LB. DROP HAMMER AND AN 18 INCH FALL. SHOULDER MATERIALS ARE INCLUDED IN THIS CATEGORY.

S-2 INCLUDES UNIFORM SANDS WHICH EXHIBIT PENETRATION VALUES OF LESS THAN 8 BLOWS PER FOOT USING A 300 LB. DROP HAMMER, BEACH SANDS ARE INCLUDED IN THIS CATEGORY.

THE GEOTECHNICAL ENGINEERING BUREAU AND THE STRUCTURES DIVISION SHOULD BE CONTACTED WHEN A POST IS TO BE PLACED IN A PLASTIC SOFT CLAY OR ORGANIC DEPOSIT, OR WHEN THE GROUNDWATER ELEVATION IS WITHIN THE MINIMUM EMBEDMENT.



TYPE A SIGN SUPPORTS

18

TYPE A SIGN SUPPORTS

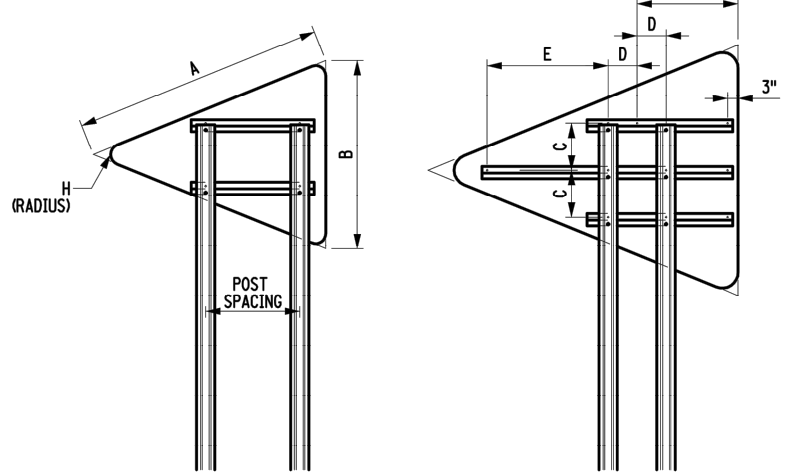
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A	B	C	H	AREA (SQ. FT.)	SEE NOTE
12"	12"	8"	1 1/2"	1.0	
18"	18"	12"	1 1/2"	2.3	
12"	6"	4"	1 1/2"	0.5	
12"	8"	4"	1 1/2"	0.7	
12"	9"	4"	1 1/2"	0.8	
18"	6"	4"	1 1/2"	0.8	
18"	12"	6"	1 1/2"	1.5	
6"	12"	6"	1 1/2"	0.5	
8"	16"	10"	1 1/2"	0.8	
9"	12"	6"	1 1/2"	1.0	
12"	18"	12"	1 1/2"	1.5	
12"	24"	18"	1 1/2"	2.0	
12"	30"	24"	1 1/2"	2.5	
12"	36"	14"	1 1/2"	3.0	2
12"	48"	18"	1 1/2"	4.0	2
12"	54"	24"	1 1/2"	4.5	2
12"	60"	24"	1 1/2"	5.0	2
18"	24"	18"	1 1/2"	3.0	
18"	30"	14"	1 1/2"	3.8	
18"	48"	18"	1 1/2"	6.0	2
18"	54"	24"	1 1/2"	6.8	2
18"	60"	24"	1 1/2"	7.5	2

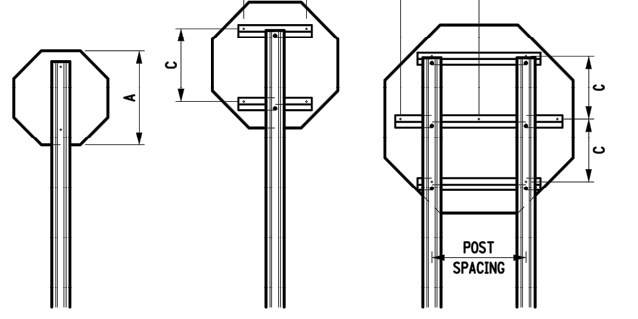
A	B	C	D	H	AREA (SQ. FT.)	SEE NOTE
21"	21"	15"	15"	1 1/2"	3.1	
12"	24"	18"	18"	1 1/2"	4.0	
30"	30"	24"	24"	1 1/2"	6.3	
20"	18"	12"	15"	1 1/2"	2.5	
21"	15"	9"	15"	1 1/2"	2.2	
24"	6"	3"	18"	1 1/2"	1.0	
24"	8"	4"	18"	1 1/2"	1.3	
24"	12"	6"	18"	1 1/2"	2.0	
24"	15"	9"	18"	1 1/2"	2.5	
24"	18"	12"	18"	1 1/2"	3.0	
30"	10"	6"	24"	1 1/2"	2.1	
30"	15"	9"	24"	1 1/2"	3.1	
30"	18"	12"	24"	1 1/2"	3.8	
30"	21"	15"	24"	1 1/2"	4.4	
30"	24"	18"	24"	1 1/2"	5.0	
24"	30"	22"	18"	1 1/2"	5.0	
24"	36"	24"	18"	1 1/2"	6.0	
30"	36"	24"	24"	1 1/2"	7.5	
24"	54"	24"	18"	1 1/2"	9.0	2
30"	42"	18"	24"	1 1/2"	8.8	2

A	B	C	D	H	POST SPACING	AREA (SQ. FT.)	PAYMENT AREA (SQ. FT.)	SEE NOTE
36"	12"	6"	15"	1 1/2"	18"	3.0		
36"	18"	12"	15"	1 1/2"	18"	4.5		
36"	24"	18"	15"	1 1/2"	18"	6.0		
36"	30"	24"	15"	1 1/2"	18"	7.5		
36"	36"	24"	15"	1 1/2"	18"	9.0		
42"	21"	12"	18"	1 1/2"	18"	6.1		
42"	26"	18"	18"	1 1/2"	18"	7.6		
42"	30"	24"	18"	1 1/2"	18"	8.8		
42"	36"	24"	18"	1 1/2"	18"	11.3		
48"	18"	12"	21"	1 1/2"	24"	6.0		
48"	24"	18"	21"	1 1/2"	24"	8.0		
48"	30"	24"	21"	1 1/2"	24"	10.0		
48"	36"	24"	21"	1 1/2"	24"	12.0		
54"	18"	12"	24"	1 1/2"	24"	6.8		
60"	24"	18"	27"	1 1/2"	30"	10.0		
60"	30"	24"	27"	1 1/2"	30"	12.5		
36"	48"	18"	15"	1 1/2"	18"	12.0	2	
36"	54"	24"	15"	1 1/2"	18"	13.5	2	
36"	60"	24"	15"	1 1/2"	18"	15.0	2	
42"	60"	24"	18"	1 1/2"	3"	24"	17.5	2
48"	60"	24"	21"	1 1/2"	3"	24"	20.0	2
42"	42"	18"	21"	1 1/2"	3"	24"	12.3	2
48"	48"	18"	21"	1 1/2"	3"	24"	16.0	2
60"	48"	18"	27"	1 1/2"	3"	30"	20.0	2
64"	42"	18"	27"	1 1/2"	3"	30"	18.7	2

A	B	C	D	E	G	H	POST SPACING	AREA (SQ. FT.)	PAYMENT AREA (SQ. FT.)
40"	30"	10"				1 1/2"	9"	3.8	7.1
48"	36"	12"				1 1/2"	12"	6.6	10.3
64"	48"	11"	6"	18"	17"	3"	12"	9.7	18.1



A	C	D	E	POST SPACING	AREA (SQ. FT.)	PAYMENT AREA (SQ. FT.)
18"	12"				1.8	2.3
24"	14"	12"			3.1	4.0
30"	18"	12"			5.0	6.3
36"	12"	18"	15"	18"	7.0	9.0
48"	16"	24"	21"	24"	12.5	16.0

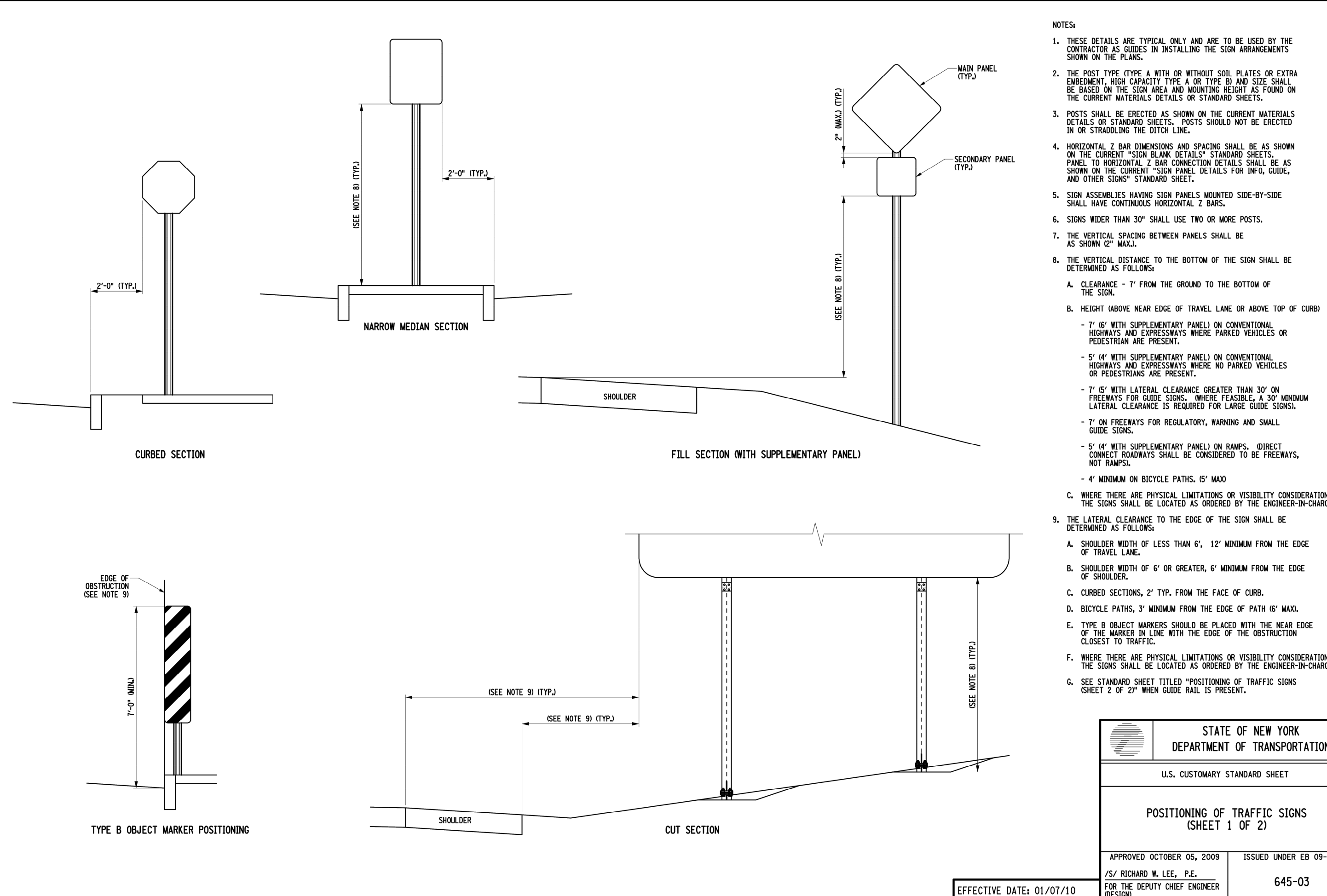


STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION	
U.S. CUSTOMARY STANDARD SHEET	
SIGN BLANK DETAILS (SHEET 1 OF 2)	
APPROVED OCTOBER 06, 2009 /S/ RICHARD W. LEE, P.E. FOR THE DEPUTY CHIEF ENGINEER	ISSUED UNDER EB 09-025 645-01
EFFECTIVE DATES: 01/07/10	

SIGN BLANK DETAILS

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POSITIONING OF TRAFFIC SIGNS



STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION	
U.S. CUSTOMARY STANDARD SHEET	
POSITIONING OF TRAFFIC SIGNS (SHEET 1 OF 2)	
APPROVED OCTOBER 06, 2009 /S/ RICHARD W. LEE, P.E. FOR THE DEPUTY CHIEF ENGINEER	ISSUED UNDER EB 09-025 645-03
EFFECTIVE DATES: 01/07/10	

POSITIONING OF TRAFFIC SIGNS

21

No.	Revision	Date
1.	REVISED FOR BIDDING PURPOSES	07/12/2018
2.	REVISED LAYOUT	05/06/2019
3.	REVISED PER NYSDOT COMMENTS	10/10/2019

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ANY ALTERATION OF PLANS,
SPECIFICATIONS, PLATS AND
REPORTS BEARING THE SEAL
OF A LICENSED PROFESSIONAL
ENGINEER OR LICENSED LAND
SURVEYOR IS A VIOLATION OF
SECTION 7209 OF THE NEW
YORK STATE EDUCATION LAW,
EXCEPT AS PROVIDED FOR BY
SECTION 7209, SUBSECTION 2.

Drawn: KRM	Approved: RJP
Scale: NOT TO SCALE	
Date: 04/27/2018	
Project No: 15119	
15119-REMS DET-4	-
Drawing No:	
HP-10	

TABLE NY1-A BARRIER VEHICLE USE REQUIREMENTS (LONG TERM, INTERMEDIATE TERM, AND SHORT TERM STATIONARY CLOSURES)			
CLOSURE TYPE	EXPOSURE CONDITION ¹	USE REQUIREMENTS ^{4,5}	
		NON-FREEMAY PRECONSTRUCTION POSTED SPEED LIMIT	POSTED SPEED LIMIT
		FREEMAY	
		≥ 45 MPH	35-40 MPH ≤ 30 MPH
LANE CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED	REQUIRED ²	REQUIRED ² OPTIONAL ²
	NON-TRANSVERSABLE HAZARD (E.G. EQUIPMENT, MATERIALS, EXCAVATION) ON NO WORKERS EXPOSED	REQUIRED ²	REQUIRED ² OPTIONAL ²
SHOULDER CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED	REQUIRED ²	REQUIRED ² OPTIONAL ²
	NON-TRANSVERSABLE HAZARD (E.G. EQUIPMENT, MATERIALS, EXCAVATION) ON NO WORKERS EXPOSED	REQUIRED ²	OPTIONAL ² OPTIONAL ²

1. THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NY1-A ASSUMES THERE IS NO POSITIVE PROTECTION TEMPORARY TRAFFIC BARRIER. WHEN WORKERS OR HAZARDS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, BARRIER VEHICLES ARE NOT REQUIRED.
2. WHERE THE REQUIREMENT IS "OPTIONAL", EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
3. REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL REMAIN IN PLACE DURING BOTH WORKING AND NON-WORKING HOURS UNTIL THE HAZARD IS REMOVED. EXCEPTION TO THESE REQUIREMENTS MAY BE MADE, AS APPROVED BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE, WHERE BARRIER VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
4. BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
5. BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.

TABLE NY1-B SHADOW VEHICLE USE REQUIREMENTS (MOBILE CLOSURES)			
CLOSURE TYPE	EXPOSURE CONDITION	USE REQUIREMENTS	
		NON-FREEMAY PRECONSTRUCTION POSTED SPEED LIMIT	POSTED SPEED LIMIT
		FREEMAY	
		≥ 45 MPH	35-40 MPH ≤ 30 MPH
LANE CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED ^{2,4}	REQUIRED ^{2,4} REQUIRED ^{2,4}
	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED ^{2,4}	REQUIRED ^{2,4} REQUIRED ^{2,4}

1. A MOBILE CLOSURE SHALL BE USED FOR ANY WORK ACTIVITY THAT MOVES CONTINUOUSLY OR INTERMITTENTLY ALONG THE TRAVELED WAY OR SHOULDER SLOWER THAN THE PREVAILING SPEED OF TRAFFIC. CHANNELIZING DEVICES ARE NOT USED FOR MOBILE CLOSURES.
2. SHADOW VEHICLES SHALL BE EQUIPPED WITH AN APPROVED REAR MOUNTED ATTENUATOR (TRUCK MOUNTED OR TRAILER MOUNTED) FOR THE FOLLOWING MOBILE CLOSURES: LANE CLOSURES ON FREEWAYS, LANE CLOSURES ON NON-FREEMAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR MORE, SHOULDER CLOSURES ON FREEWAYS, AND SHOULDER CLOSURES ON NON-FREEMAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE.
3. FOR MOBILE LANE CLOSURES ON NON-FREEMAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR LESS AND MOBILE SHOULDER CLOSURES ON NON-FREEMAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 40 MPH OR LESS, SHADOW VEHICLES ARE NOT REQUIRED TO BE EQUIPPED WITH A REAR MOUNTED ATTENUATOR.
4. A SHADOW VEHICLE IS USED TO PROTECT EXPOSED WORKERS ON FOOT OR IN A VEHICLE AND SHALL BE REQUIRED FOR ALL MOBILE CLOSURES. SHADOW VEHICLE REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE SHADOW VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. ADDITIONAL REQUIREMENTS MAY BE REQUIRED FOR MOBILE CLOSURES ON NON-FREEMAY ROADWAYS AND THE INCREASED PROTECTION OF EXPOSED WORKERS AS DIRECTED BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE.

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS									
SPEED LIMIT (SI) MPH	TAPER LENGTH (L) FT		L = TAPER LENGTH P = WIDTH OF OFFSET (FT) S = PRECONSTRUCTION POSTED SPEED LIMIT (MPH)						
	L = WS ² / ADO		L = WS						
140 MPH OR LESS	L = WS ² / ADO		L = WS						
45 MPH OR MORE	L = WS		L = WS						

TABLE 6C-2 LONGITUDINAL BUFFER SPACE	
PRECONSTRUCTION POSTED SPEED (LIMIT (MPH)	DISTANCE
25	155 F.T.
30	165 F.T.
35	175 F.T.
40	185 F.T.
45	195 F.T.
50	205 F.T.
55	215 F.T.
60	225 F.T.
65	235 F.T.
70	245 F.T.
75	255 F.T.
80	265 F.T.
85	275 F.T.